

** Bloomfield Center Redevelopment Plan - Phase I*



Township of Bloomfield, New Jersey

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Introduction

- 1.1 Background
- 1.2 Redevelopment Plan Boundary

Existing Conditions



Block 228



Block 243



Farrand Street Parking Lot



Royal Theatre Parking Lot

Bloomfield Assets



2 Broad Street



Venner Park



Post Office



The Green



Bloomfield College



Bloomfield Lackawanna Station



Watsessing Park



Town Hall

The Future of Bloomfield



Attractive Retail



New Development



Pocket Parks



Sustainability

The Bloomfield Center Redevelopment Plan - Phase I is based on the following planning principles:

- Implementing the core concepts of the “triple bottom line” of economic, social, and environmental sustainability
- Capitalizing on mass transit opportunities
- Revitalizing the commercial district
- Promoting a mixture of land uses
- Integrating efficient structured parking “wrapped” with mixed-use development
- Providing a pedestrian-oriented environment
- Rationalizing traffic circulation with a “loop road” around the downtown
- Utilizing high quality design standards

Background

“Downtown” Bloomfield, which is centered on the historic “Six Points” intersection, has long served as a transportation hub and downtown shopping district for residents, commuters, and people within the region. However, for the past 25 years, the downtown has been in decline. The area has long been identified as Bloomfield Center. The Bloomfield Center Special Improvement District (“SID”) was established in June 2001. Since that time the “Bloomfield Center Alliance” (“BCA”) has been managing this district and has made significant efforts to revitalize this area. This redevelopment plan will build upon this identity to position the downtown as the thriving center of Bloomfield.

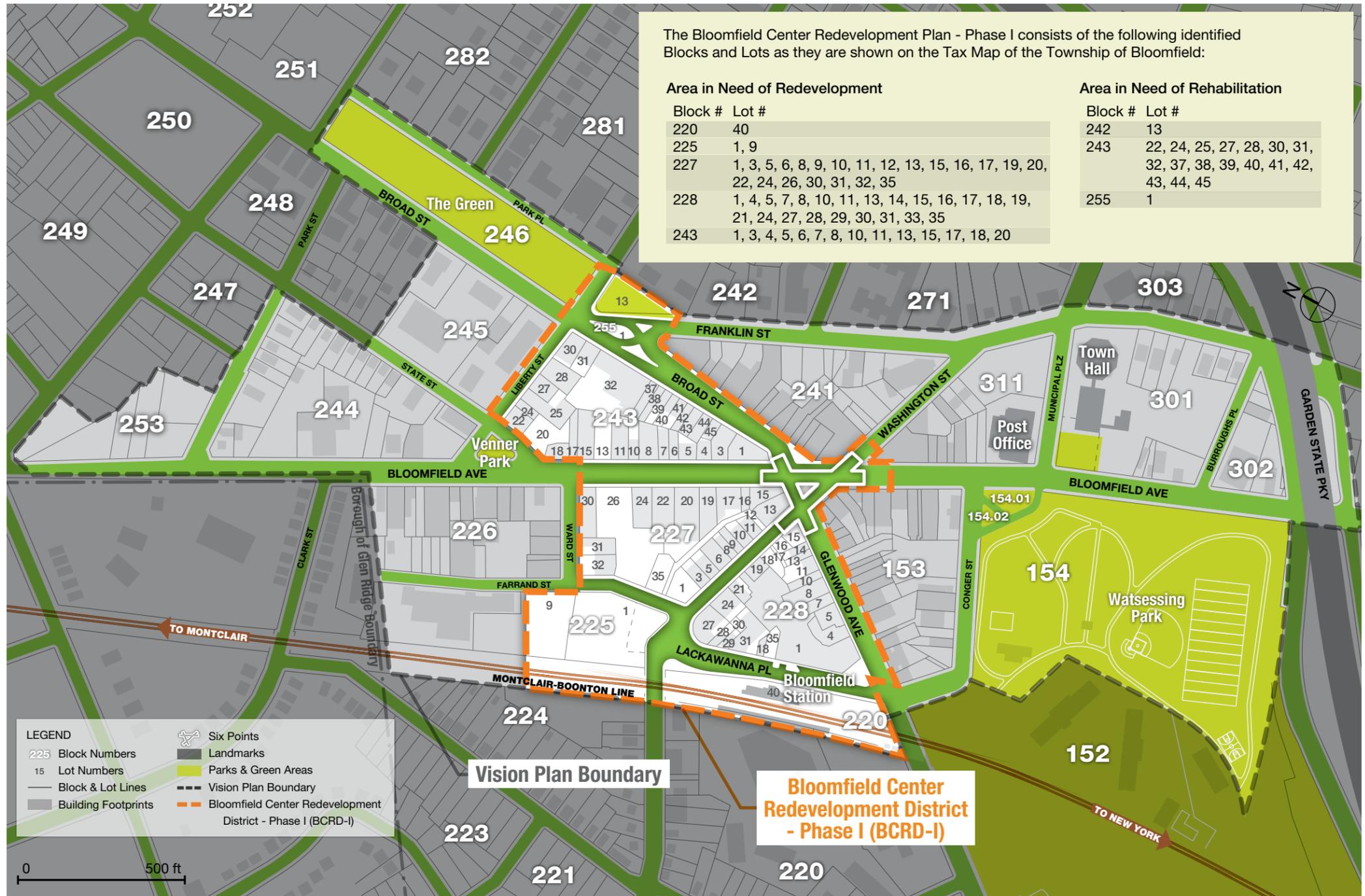
The “Bloomfield Center Redevelopment Plan - Phase I” (“BCRP-I”) will be a new redevelopment plan that comes as a result of failed redevelopment attempts that began in 2000 in response to the steady decline of the downtown. In 2000, the Township of Bloomfield Planning Board formally declared the Bloomfield Lackawanna Station and surrounding area as “an area in need of redevelopment” to spur revitalization and redevelopment. Subsequently, in 2003, the Bloomfield Station Redevelopment Plan (“BSRP”) was adopted.

Due to unanticipated occurrences and legal actions with regards to redevelopment efforts that were to be undertaken by Forest City Bloomfield, L.L.C. and Forest City Bloomfield Urban Renewal, L.L.C., with respect to the Bloomfield Station Redevelopment Plan, but which did not materialize, the Mayor and Council of the Township of Bloomfield (“Mayor and Council”) conducted a series of community planning meetings. The Mayor and Council actively pursued comments from residents, property owners, potential redevelopers and other interested citizens to participate in the formulation of this new Redevelopment Plan. The goal of the new plan is to recognize the interests and rights of Bloomfield property owners and residents.

This “BCRP-I” will address significant portions of the downtown, including the Bloomfield Station Redevelopment Area (with the exception of Block 220, Lots 26, 27, 30 and 35, which are now or formerly owned and controlled by 110 Washington Street Associates, and which shall continue to be governed by the Bloomfield Station Redevelopment Plan), and a portion of areas included in the 2001 Township-wide Rehabilitation Area designation. Simultaneously, the Bloomfield Parking Authority (“BPA”) is undertaking a downtown-wide parking plan, which will ultimately include the construction of new structured public parking.

Background

1.1



Redevelopment Plan Boundary

The Bloomfield Center Redevelopment Plan - Phase I is the first step towards the implementation of "The Vision Plan" as described in "The Bloomfield Center Redevelopment Vision" section of this plan. The Vision Plan Boundary, as identified in Exhibit 3, encompasses the logical boundary for the redevelopment of Bloomfield Center as identified in the community visioning process. It is anticipated that future redevelopment plan(s) will be created for the remainder of this Vision Plan Area.

Once adopted, The Bloomfield Center Redevelopment Plan - Phase I creates The Bloomfield Center Redevelopment District - Phase I ("BCRD-I"). The "BCRD-I" includes two distinct area designations:

1. Parcels found to be "in need of redevelopment" per the Resolution adopted by the Mayor and Council on August 7, 2006; and
2. Parcels designated as "area in need of rehabilitation".

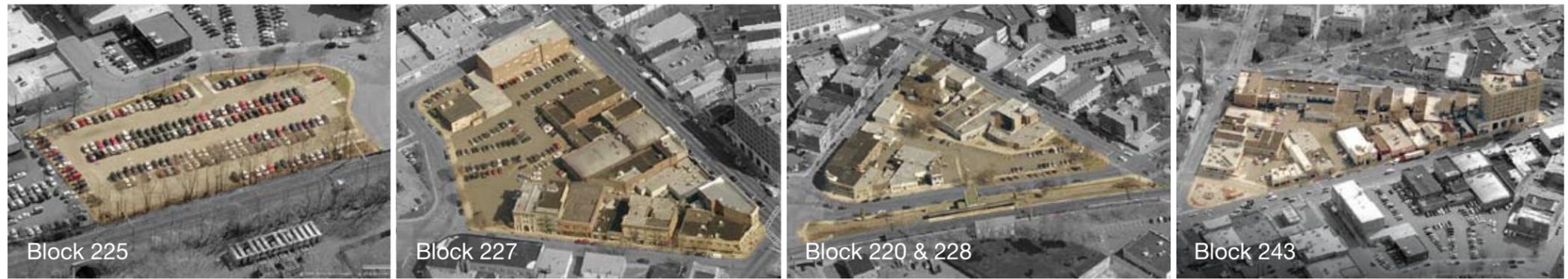
The area addressed in this Redevelopment Plan includes both a designated "area in need of redevelopment" and a portion of a designated "area in need of rehabilitation", in accordance with the Local Redevelopment and Housing Law (N.J.S.A. 40A:12A-1 et. seq.)

The BCRD - I is therefore bounded to the north by Liberty Street, to the south by the NJ Transit train tracks, to the east by Broad Street and Glenwood Avenue and to the west by Ward Street and State Street. The area is comprised of a variety of uses including commercial and residential, but is dominated by retail uses. Bloomfield Avenue, a major regional connector, runs through the center of the area. Bloomfield Avenue's intersection with Broad Street, Glenwood Avenue and Washington Street form the historic 'Six Points' intersection, a local landmark. NJ Transit's Bloomfield Station is located on the southern edge of the area and provides direct access to Midtown Manhattan and Hoboken.

The BCRD-I is identified on the Zoning District Map. The Zoning District Map is attached as Schedule A, attached hereto and made part hereof.

Exhibit 3. Redevelopment Plan Boundary Map

Refer to Schedule A for precise location of "BCRD-I" boundary



Redevelopment Plan Boundary

1.2

The Bloomfield Center Redevelopment Vision

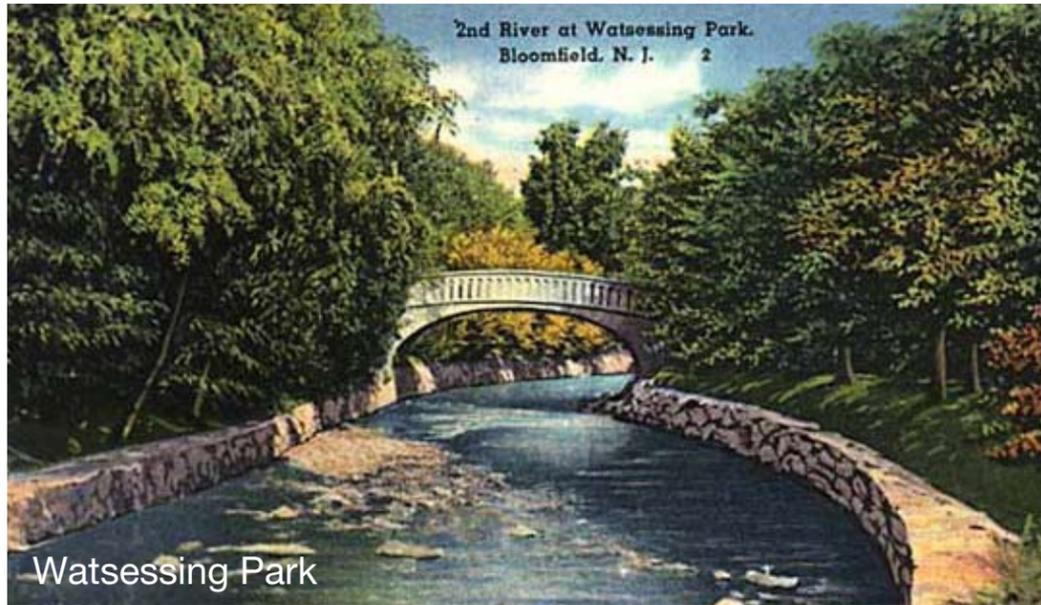
Bloomfield Center Redevelopment

As a first step towards revitalizing “Downtown” Bloomfield, a community driven Vision Plan was developed. Initiated by the Mayor and Council of the Township of Bloomfield, a series of community workshops were held throughout the fall and winter of 2007. These workshops covered several topics including:

- **General Community Goals:** Residents were asked to provide input regarding a variety of topics including parking locations, downtown assets, streetscape elements and pedestrian circulation.
- **Shaping the Downtown:** This workshop focused on the elements that influence the “look and feel” of the downtown including: Building Form (heights and stepbacks), Storefronts and Signage, Streetscape and Urban Design, and Green Building.
- **Architecture:** Three topics were discussed in greater detail in small group settings. These topics were: Rehabilitation and Infill, Architectural Style and Architectural Form and Roofscape.

The workshops culminated in the development of the Bloomfield Center “Vision Plan”, presented on April 9, 2008. The Vision Plan outlines:

- 2.1 Vision Statements
 - A Historic Urban Center
 - New Jersey Urban Lifestyle
 - A Transit Village
 - A Green Downtown
 - An Accessible Downtown
- 2.2 Reconnecting the Downtown
- 2.3 Place Making
- 2.4 Concept Plan



2nd River at Watsessing Park.
Bloomfield, N. J. 2

Watsessing Park



2 Broad Street



Six Points

A Historic Urban Center

“Celebrate the rich history of Bloomfield while looking towards the future...”

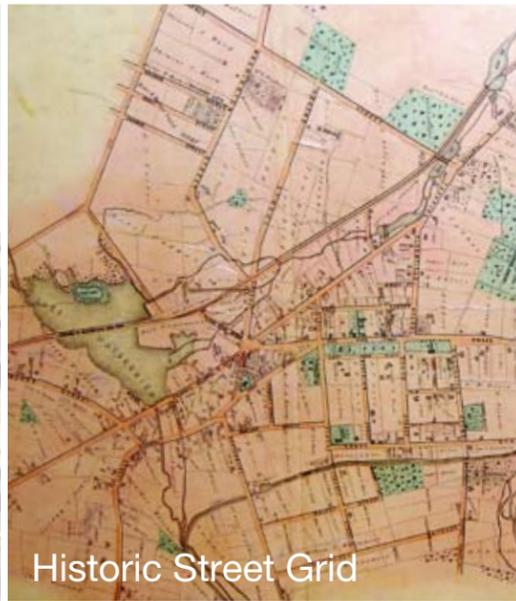
Bloomfield is one of New Jersey’s great historic suburban downtowns. Through the mid-1900’s Bloomfield Center was a bustling shopping destination serving residents and visitors from surrounding towns. Goods ranging from day-to-day needs to clothing and shoes could all be found in Bloomfield Center.

The downtown’s role as the civic, shopping and community hub has steadily declined as suburban growth extended outward. With the popularity of shopping centers and malls, downtown Bloomfield suffered the same fate as many of New Jersey’s great urban cores. Nonetheless, Bloomfield has maintained its strong identity and valuable assets that span the history of the town, such as Two Broad Street, a Bloomfield symbol, the historic Town Hall, the buildings of Bloomfield College, the urban streetscapes, the historic Green and the historic train station. A key characteristic is its historic diagonal street grid, which creates interest but has become a barrier to development.

The intersection of Bloomfield Avenue, a regional connector, with Broad Street and Washington Street forms the center of the downtown, known as “6-Points”. With the historic train station and new link to mid-town Manhattan only one block from “6-Points”, Bloomfield’s urban infrastructure is poised to support a downtown revival. This Redevelopment Plan seeks to revitalize downtown Bloomfield by capitalizing on the assets and infrastructure inherent to this historic urban core and infuse it with the modern-day amenities that residents and shoppers desire.



Urban Architecture -
Broad Street



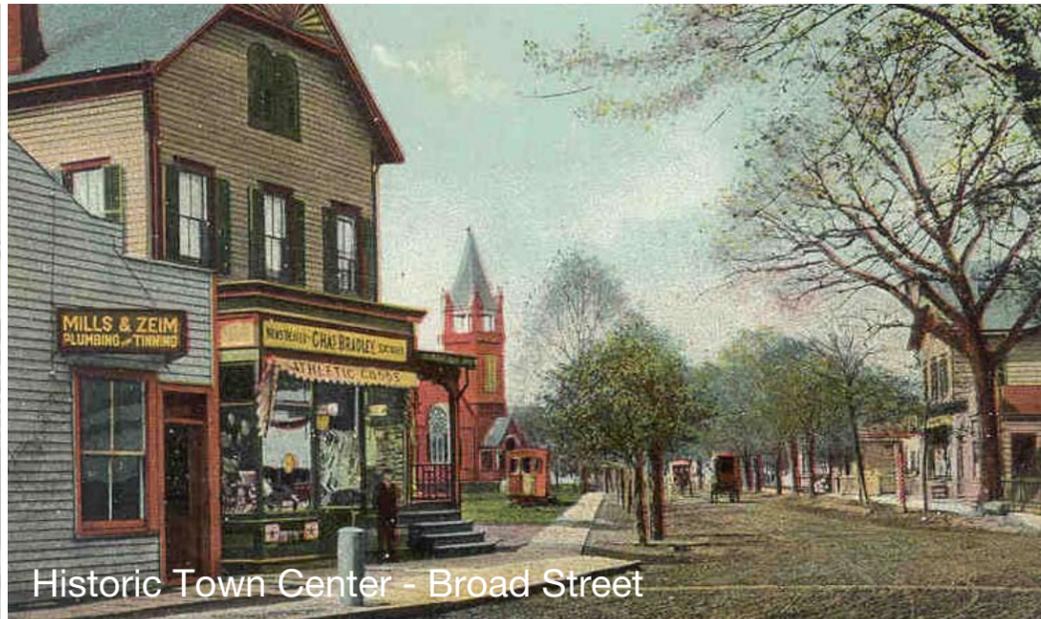
Historic Street Grid



Urban Façade - Bloomfield Avenue



Retail Streetscape -
Bloomfield Avenue



Historic Town Center - Broad Street



Train Station



Town Green
Morristown, NJ



Event Street
Montclair, NJ



Historic Character
Morristown, NJ



Farmers Market
New Brunswick, NJ



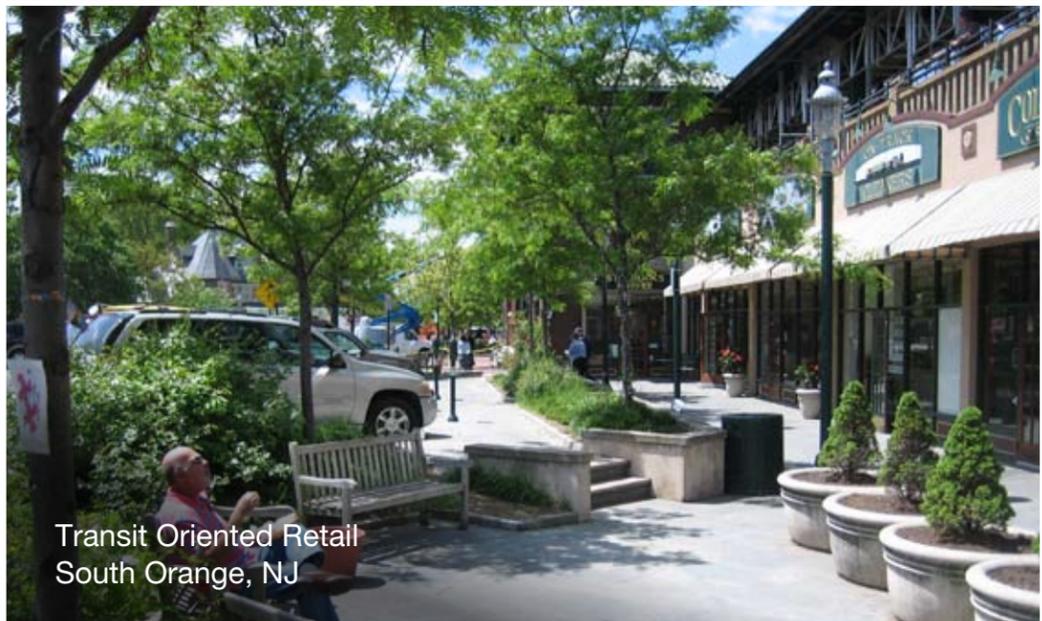
Pedestrian Safety
Montclair, NJ



Mixed-Use
South Orange, NJ



Compact Development
New Brunswick, NJ



Transit Oriented Retail
South Orange, NJ

New Jersey Urban Lifestyle

“Capitalize upon the existing infrastructure and urban character inherent to downtown Bloomfield..”

The Township of Bloomfield offers residents and visitors the unique benefits of both the urban and suburban lifestyle, which is characteristic of some nearby towns that have seen revivals. Accessibility and convenience are at the core of the New Jersey urban lifestyle. Convenient access to jobs, culture and shopping in adjacent cities New York and Newark is key. People around the country are seeking out the characteristics of living, working and shopping environments that Bloomfield offers.

People want:

- A sense of community
- A variety of housing types ranging from single family homes to apartments
- The ability to walk and bike to daily conveniences
- Ample parks, open space and community facilities;
- Community events
- A healthier and more sustainable environment with trees and greenery, fresh air, natural lighting and clean streets

In addition, Bloomfield’s central location and good highway access enables it to be a regional destination and create diverse shopping and cultural experiences for residents and visitors.

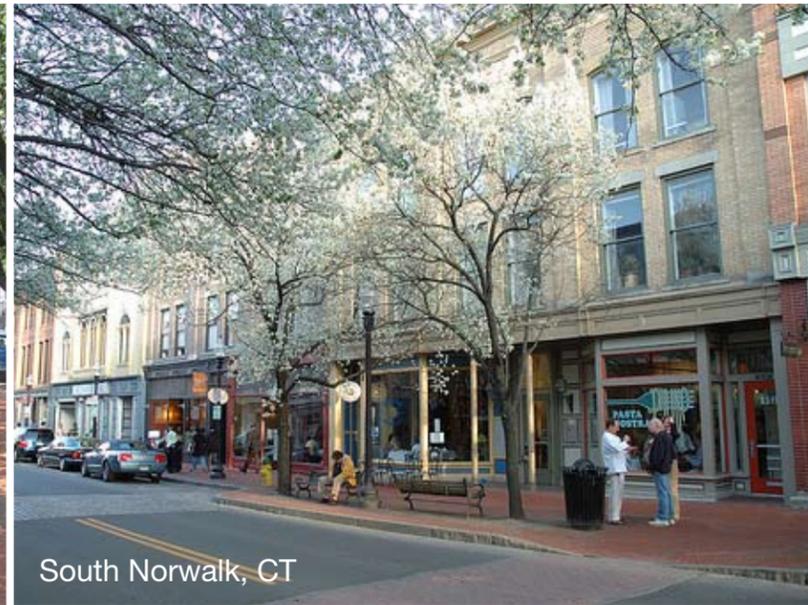
Municipalities locally and nationally have capitalized on their “urban lifestyle” to provide a “richness” to suburban living. This Redevelopment Plan will build upon the traditional downtown qualities adapted to meet current market dynamics and trends.



South Norwalk, CT



South Norwalk, CT



South Norwalk, CT



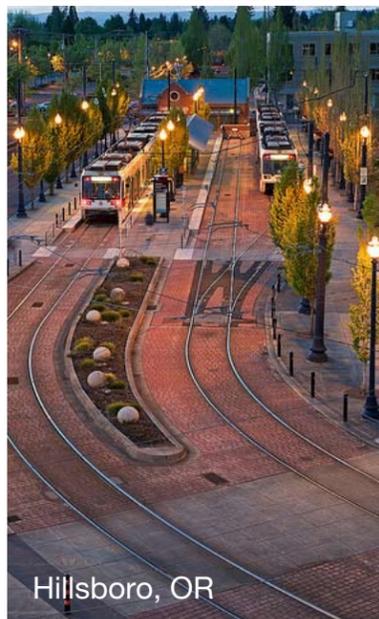
Redwood City, CA



Redwood City, CA



Redwood City, CA



Hillsboro, OR



Hillsboro, OR



Hillsboro, OR



Tarrytown, NY



Tarrytown, NY

A Transit Village

“Encourage the development of a pleasant, compact urban environment well served by rail and bus transit, to encourage walking and minimize car dependency..”

A Transit Village is a community well served by transit, reducing dependency on cars - by riding transit and then walking through pleasant pedestrian environments. Transit Villages are active, vibrant, and walkable neighborhood centers and typically contain a higher density of residential housing. Typically, any area within a 5-10 minute walk of the station can take advantage of the benefits of train access to enhance its potential for residential living, shopping or office uses. Bloomfield’s entire downtown is within that distance of the train station.

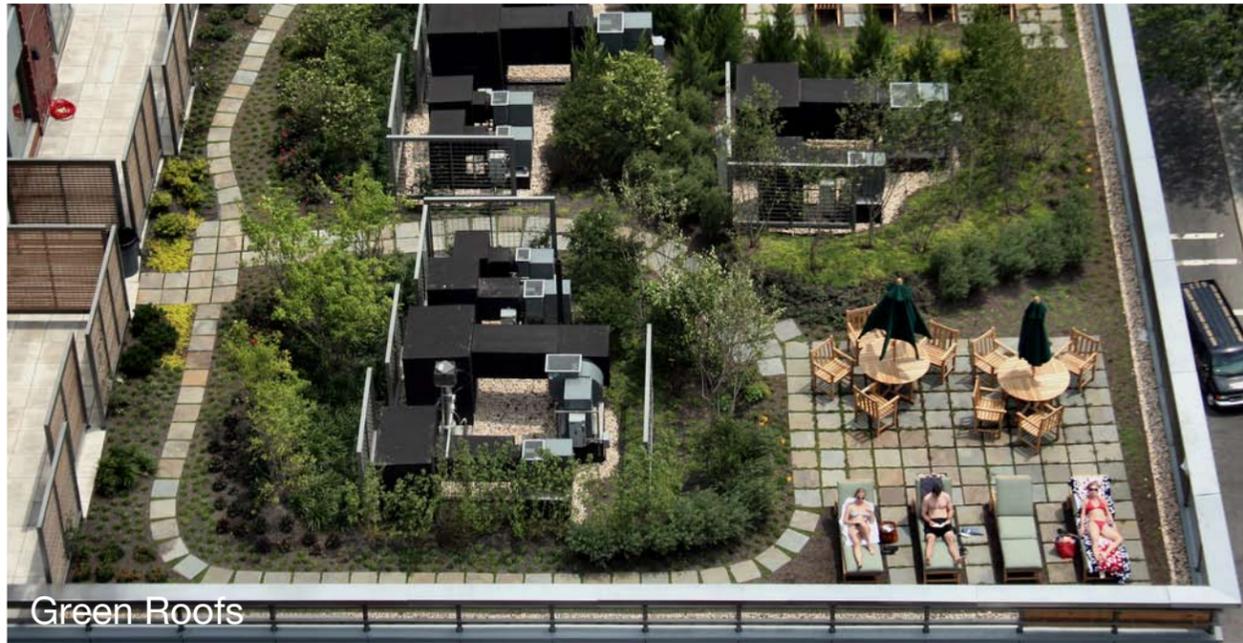
Bloomfield Center was recognized for embodying these characteristics and was granted ‘Transit Village Status’ by the state of New Jersey in 2003. Bloomfield Station, on the Montclair Boonton Line, offers express service to Midtown Manhattan, making it one of the most convenient towns of its kind in the region.

The challenge starts with the location of the train station itself, which despite its proximity, is disconnected from the heart of the downtown. A revival of the train station area, and the linking of public improvements with new private development, will reconnect the downtown. The areas that currently seem distant to the train station, such as the Town Hall and the Bloomfield College Campus area will seem very close, triggering further revitalization.

Ultimately, a Transit Village vision will yield a plan that encompasses all of the characteristics of a traditional town center, with a contemporary vitality created by the strong connections to the surrounding towns and region.



Stormwater Management



Green Roofs



Plantings



Solar Panels



Energy Efficiency



Alternative Transportation



Open Space



Dark Sky Lighting

A Green Downtown

“Ensure that all future development is sustainable, healthy and minimizes impacts to the environment...”

A “Green Downtown” is a means to invoke a comprehensive set of sustainable development principles. Sustainability is defined as meeting the needs of the present without compromising the ability of future generations to meet their own needs. The “Triple Bottom Line” is a broad approach to integrating green and sustainable practices into development projects, as follows:

- **Economic Sustainability:** produce an environment that is financially attractive for new and existing businesses, residents, visitors, and the township, with good jobs, good destinations, lower long-term operating costs, and an improved tax base.
- **Social Sustainability:** provide a livable, healthy, attractive and inclusive community
- **Environmental Sustainability:** reduce overall demands on natural resources, local infrastructure systems, and sources of pollution

As momentum around ‘green building’ grows both locally and nationally, Bloomfield has the unique ability to be a leader in this movement. The environmental challenges particular to Bloomfield are congestion, air quality, flooding, water usage and energy costs. The State of New Jersey, as an essentially “built-out” state, has adopted a Smart Growth approach to support its future growth and improvement needs.

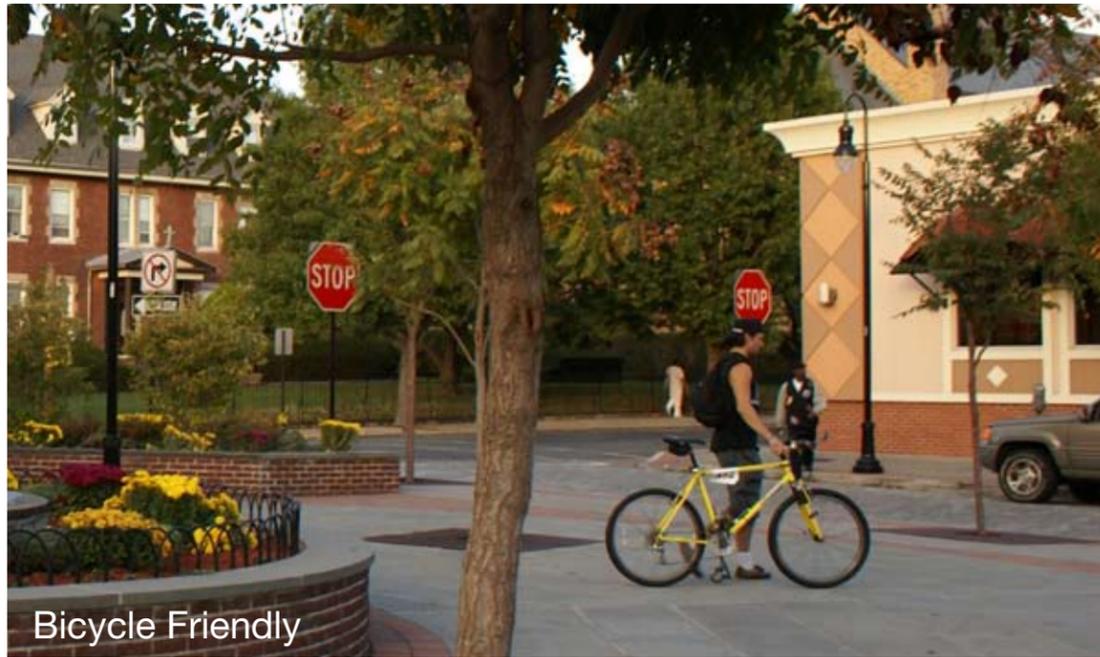
The approach for Bloomfield Center will follow New Jersey’s Smart Growth strategies as the basis of the Redevelopment Plan, shaping the plan to take full advantage of the existing sustainable features of the redevelopment area including:

- The proximity to Bloomfield’s train station and the Garden State Parkway
- The existing building stock
- Watsessing Park and The Green
- The existing density

The plan will address specific goals, strategies and approaches that are restrictive but provide multiple incentivized options for developments to achieve or exceed the Township’s goals.

Vision Statement

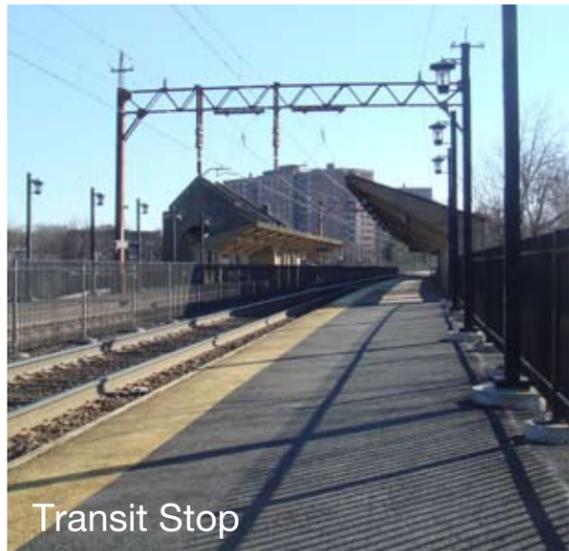
2.1



Bicycle Friendly



Regional & Local Bus Line Connections



Transit Stop



NJ Transit Service



Park Location



Safe Pedestrian Crossings



Regional Access to NYC



Highway & Regional Road Access

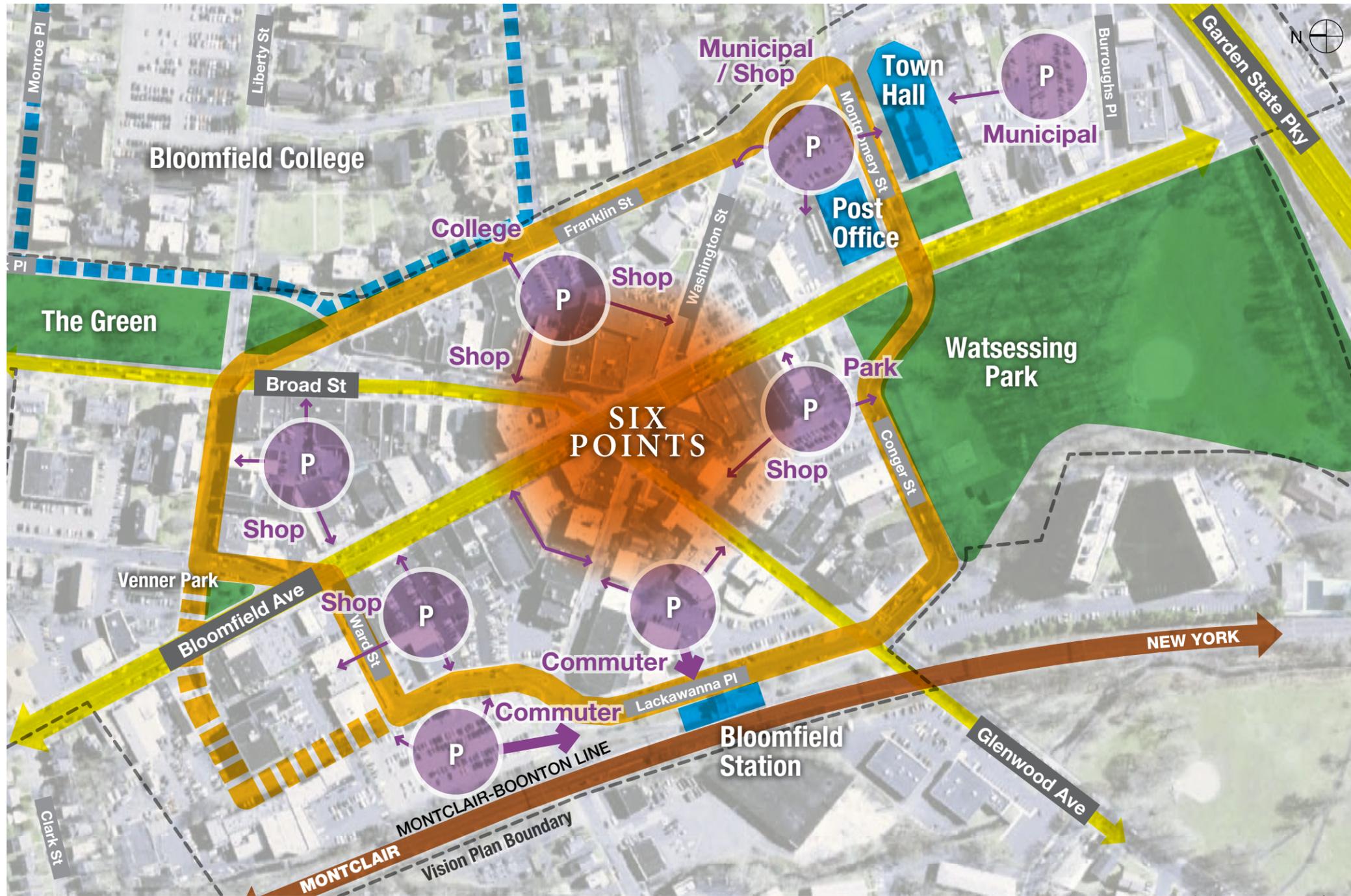
An Accessible Downtown

“Offer multiple, convenient transportation options for visitors, residents and commuters...”

A successful downtown needs to provide a great pedestrian environment. Still vehicular, train and bus accessibility and convenience will define a town’s commercial success. Despite numerous examples of downtown and town center revivals, New Jersey remains a largely suburban place where the car “rules” and development trends have followed. The challenge to Bloomfield is overcoming long held beliefs by retailers, businesses and potential residents that a more sustainable mix of transportation modes works and contributes to a more vital downtown.

Bloomfield has a rare combination of transportation assets that will enable it to overcome these challenges and follow smart growth trends where “Main Street” is back in. In addition to the train station, Bloomfield Center has great highway and regional roadway access as well as ample bus service. It is critical that each redevelopment site capitalize on these assets. This will be achieved through a coordinated public investment plan linked to development projects that benefit from targeted functionality, convenience and experience enhancements.

This Redevelopment Plan proposes enhancements and development densities that will capitalize on the existing infrastructure to create a downtown that maximizes accessibility through an integrated set of transportation modes including walking, bicycling, cars, bus and train.



Reconnecting the Downtown

Through the Community Workshop process, a series of broad public improvements were identified that will facilitate the redevelopment of Bloomfield Center. These improvements are outlined below and should be incorporated into the individual redevelopment plans as they are developed.

Downtown Assets
Focus public improvements to “reconnect” and capitalize upon the many assets that exist in downtown.

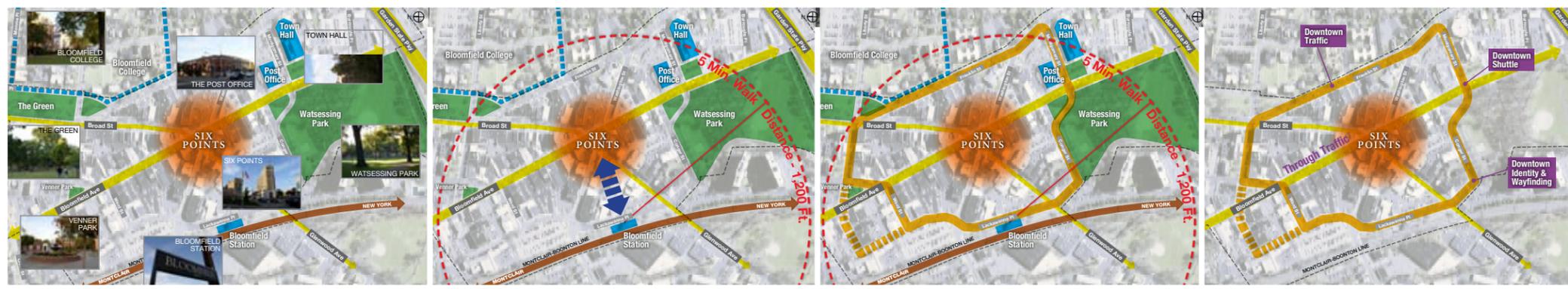
Transit Access
Capitalize upon the “Transit Village” status by providing clear vehicular, bicycle and pedestrian access to the train station. Public spaces such as plazas and parks focused around the train station will further enhance this goal. In addition to train station improvements, new and improved bus stops and neighborhood shuttles will draw bus riders back into the retail areas of downtown.

Vehicular Access and Circulation
A vehicular “Loop Road” will offer visitors clear circulation around the downtown. This loop road will reduce traffic on Bloomfield Avenue and provide safe, easy access to public parking facilities.

Public Parking
A series of well distributed public parking structures will offer convenient parking to all downtown areas and assets. A shared parking program will maximize efficiency and offer parking to a variety of users including commuters, shoppers and residents.

Walkability and Bikability
Comfortable and safe pedestrian streets and sidewalks will create a walkable downtown and reduce reliance on automobiles and parking structures. As the center of the downtown, Six Points will become a “pedestrian hub”. With the addition of safe, well lit mid-block pedestrian alleys and connections, visitors will have convenient and interesting ways of walking throughout the downtown. Bicycle lanes and conveniently placed bike racks will also encourage riders from intermediate distances to participate in the downtown experience without driving.

Exhibit 1. Images from Public Visioning Sessions



Downtown Assets

Transit Access

Walkability

“The Loop”

The Bloomfield Center Redevelopment Plan - Phase I represents the first step towards the implementation of this vision. It is anticipated that additional redevelopment plan(s) will be established for the remaining portions of the Vision Plan Area to ensure the consistent implementation of this Vision.

Reconnecting
the
Downtown

2.2

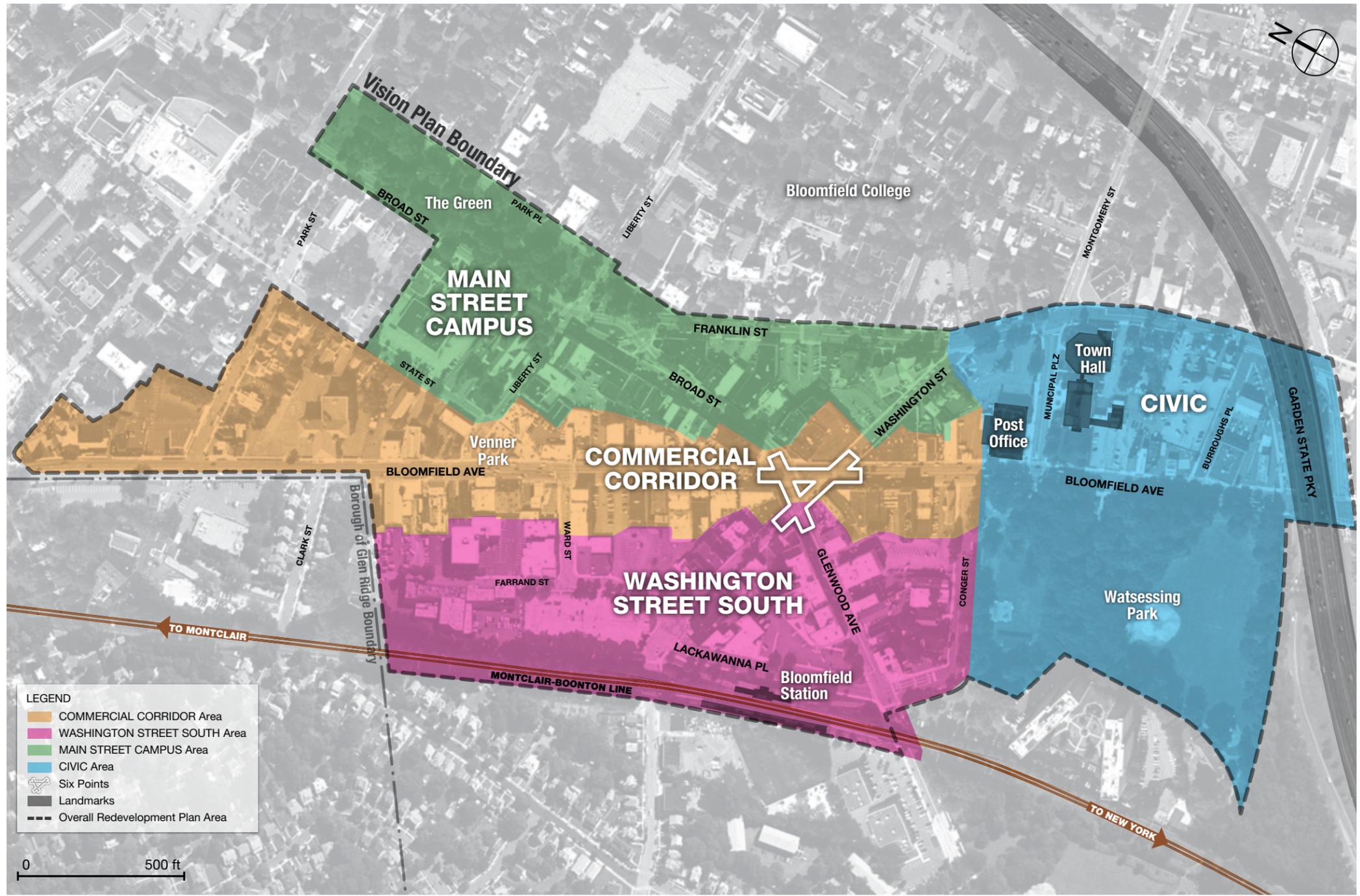


Exhibit 2. Place Making Map

Refer to Schedule A for precise location of "BCRD-I" boundary

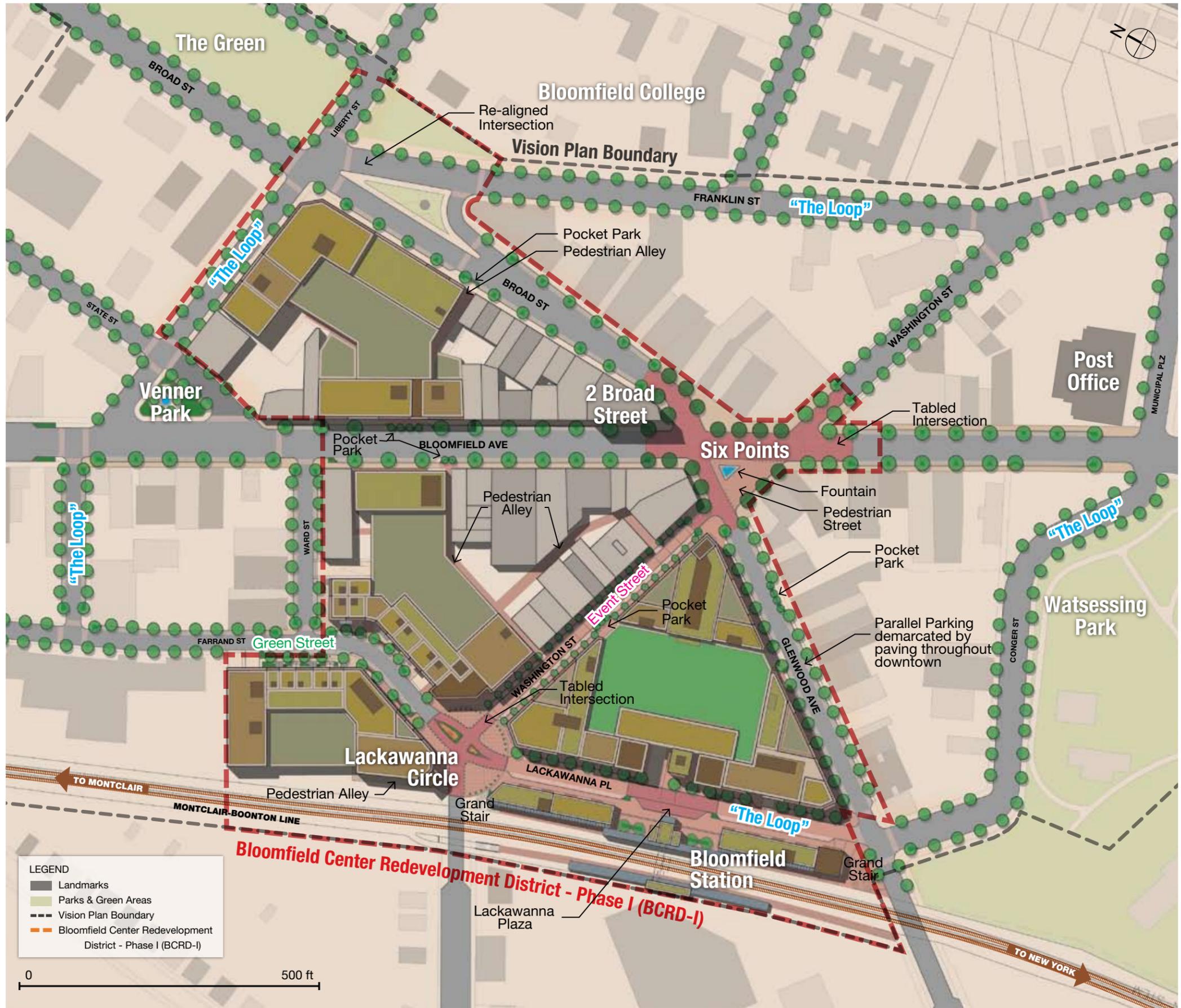
Place Making

Overview

The Vision Plan identifies four "Place Making Themes" that are based on the town's historical uses, current uses and future aspirations. The four areas – Washington Street South, Commercial Corridor, Main Street Campus and Civic District – collectively make up an eclectic set of interesting experiences with many of the elements of a successful downtown. Each offers a unique experience for visitors while maintaining a unified, seamless feel through the common streetscape elements and the historic context. The four "Place Making Themes" are:

- Commercial Corridor
- Washington Street South
- Main Street Campus
- Civic District

This redevelopment plan includes three of these four "Place Making Themes" areas: The commercial corridor, Washington Street South and Main Street Campus.



Concept Plan

The Vision Plan presentation illustrates two downtown development concept plans: High Impact Plan and Low Impact Plan. The Low Impact Plan assumes a moderate amount of property assemblage and includes properties that are either township-owned or properties where the owner has expressed interest in redevelopment. The High Impact Plan includes properties that have development potential but the owners have not yet expressed interest.

With these plans as a basis, an Illustrative Concept Plan (Exhibit 4) was developed for the Bloomfield Center Redevelopment District - Phase I. This plan illustrates the potential development opportunities as well as parks, open space and streetscape improvements. This plan is intended to illustrate the potential for development in Bloomfield Center, as outlined in The Vision Plan, and is in no way "final" or restrictive. This Illustrative Concept Plan (Exhibit 4) is flexible and will change and evolve organically as this redevelopment process moves forward.

Key Map



Concept Plan
2.4

Exhibit 4. Illustrative Concept Plan

Refer to Schedule A for precise location of "BCRD-I" boundary

Public Improvements

The Bloomfield Center Redevelopment Plan - Phase I will serve to guide the Township of Bloomfield's interests in the creation of a public/private solution for the redevelopment. A key driver of the plan will be the link between public improvements and private developments. This plan considers public parking facilities a critical public initiative along with streetscape and public space.

- 3.1 Streets and Parking
- 3.2 Transit Access
- 3.3 Public Spaces

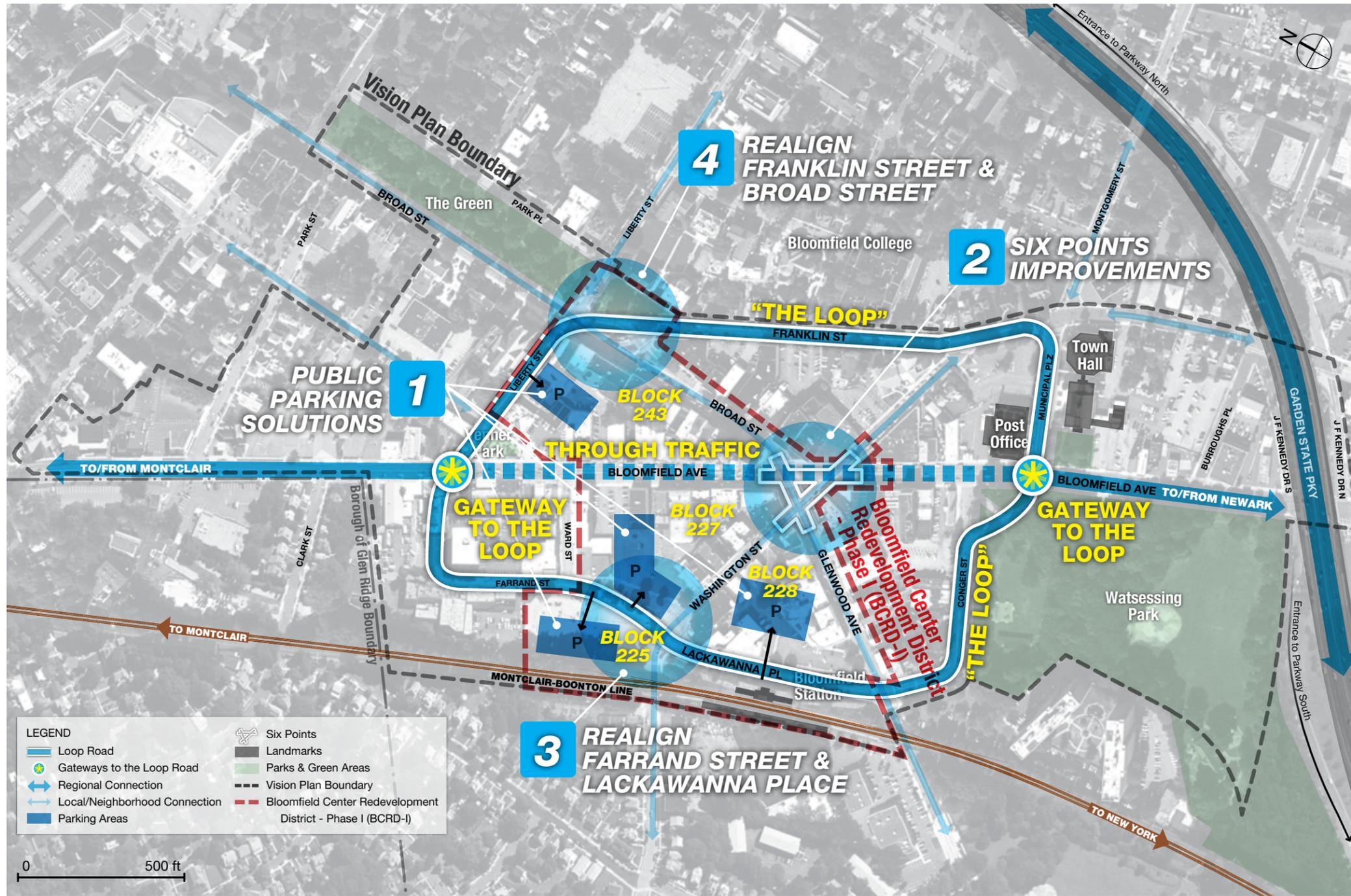


Exhibit 5. Streets and Parking

Refer to Schedule A for precise location of "BCRD-I" boundary

Streets and Parking

Bloomfield Center is comprised of a historic and unique street grid that brings together several streets at various angles to form the "Six Points" intersection. Although this street network adds to the uniqueness of Bloomfield Center, it creates a roadway system that is challenging to navigate and can be disorienting for visitors.

The Bloomfield Center Vision Plan identified a roadway system that addresses this challenge. A "Loop Road" will link a system of shared public parking structures distributed throughout the downtown is proposed. This strategy will help alleviate vehicular traffic on Bloomfield Avenue and provide easier access to Bloomfield Center. This Redevelopment Plan encompasses portions of the proposed Loop Road.

Public and Commuter Parking – A Distributed, Shared-Parking Strategy

The Bloomfield Center Vision Plan outlines a strategy for the implementation of "distributed shared parking" that will be located throughout the downtown to provide convenient access to all areas of the downtown. The strategy is to provide parking structures that will be "shared parking facilities" that will accommodate retail, residential, office and commuter uses. Of priority is providing ample commuter parking for Bloomfield Station.

During one of several public workshops, all attendees were asked to select which locations in the downtown would be most convenient for a shared parking structure. This exercise resulted in Block 228 receiving significantly more votes than any other areas due to its proximity to the both the train station and Bloomfield Avenue.

Following the public workshop, a parking analysis was performed by The Bloomfield Parking Authority. Three priority sites were identified for receiving public parking structures, located within the BCRD-I, with a fourth to be studied further. Appropriate traffic improvements and signage should be provided to ensure safe and efficient access to these proposed structures. This signage system should build upon the current gateway and wayfinding signs developed by the BCA.

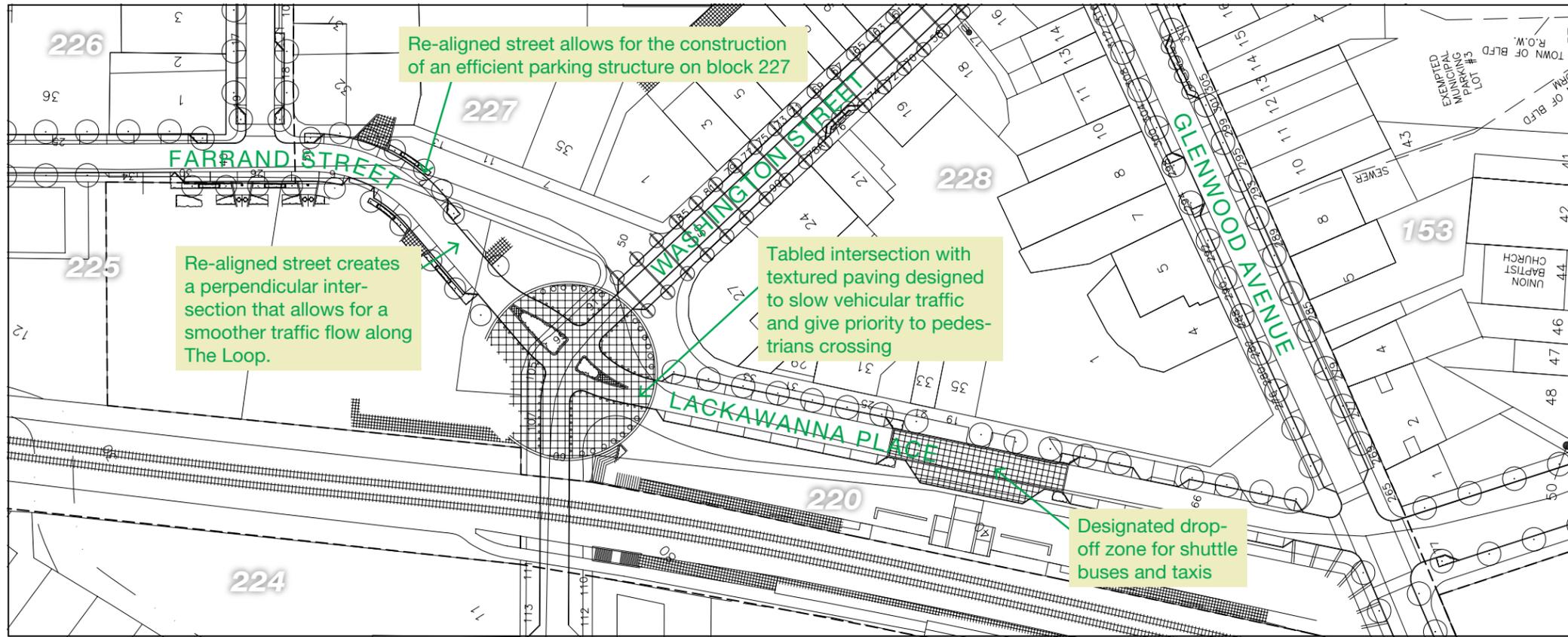


Exhibit 6. Re-align Farrand Street and Lackawanna Place

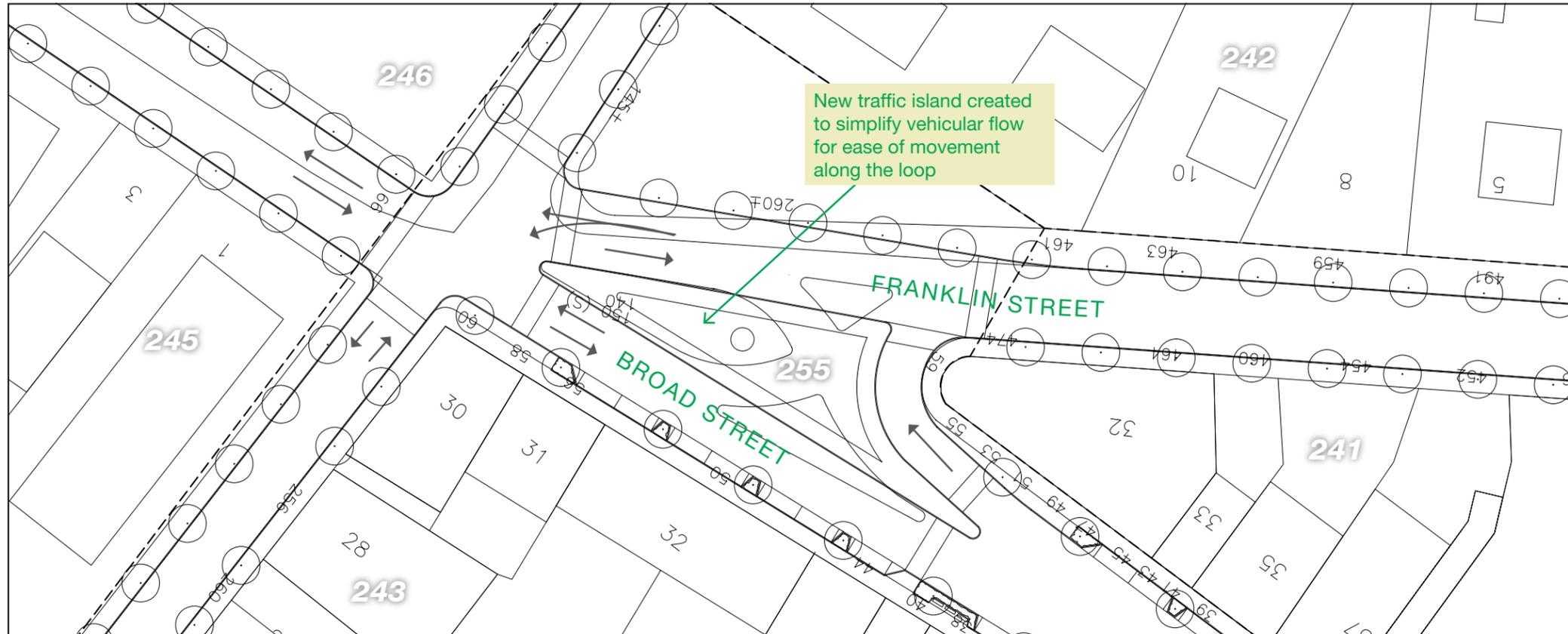


Exhibit 7. Re-align Franklin Street and Broad Street

Streets and Parking

(continued)

The following intersection improvements are recommended:

1. Public Parking Solutions

On the previous page the overall parking strategy was outlined. The following are the specific implementation blocks: Block 228 (primary commuter parking lot due to adjacency to train station), Block 227 and Block 225. In addition, lot 243 was identified as a potential site but will require further study by the BPA. BPA is separately proceeding to evaluate appropriate parking facilities to serve the multiple needs of Bloomfield Center.

2. Six Points Improvements

Six Points transformed into a “pedestrian friendly intersection”. This Redevelopment Plan recommends that the small section of Washington Street, between Bloomfield Avenue and Glenwood Avenue, be closed to motor vehicle traffic. This concept builds upon initial traffic studies and concepts developed by the BCA in 2003.

3. Realign Farrand Street and Lackawanna Place (Exhibit 6)

The intersection of Farrand Street and Lackawanna Place realigned to allow for better circulation on “The Loop”. This shift of Farrand Street will also allow for the efficient construction of a shared parking deck on the town owned property on Block 227.

This realignment gives priority to vehicles on the loop and encourages vehicles travelling north on Washington Street to make a right or left onto “The Loop”. This will ensure smooth traffic flow on “The Loop” and minimize vehicular traffic on Washington Street between the train tracks and Bloomfield Avenue. This concept builds upon initial traffic studies and concepts developed by the BCA in 2003.

4. Realign Franklin Street & Broad Street (Exhibit 7)

The intersection of Franklin Street and Broad Street realigned to simplify traffic flow and emphasize the circulation of ‘The Loop’ traffic. The triangular traffic island created by the new circulation pattern should be appropriately landscaped with low maintenance plant materials. It should also provide paved areas that act as pedestrian refuge areas for people crossing this intersection.

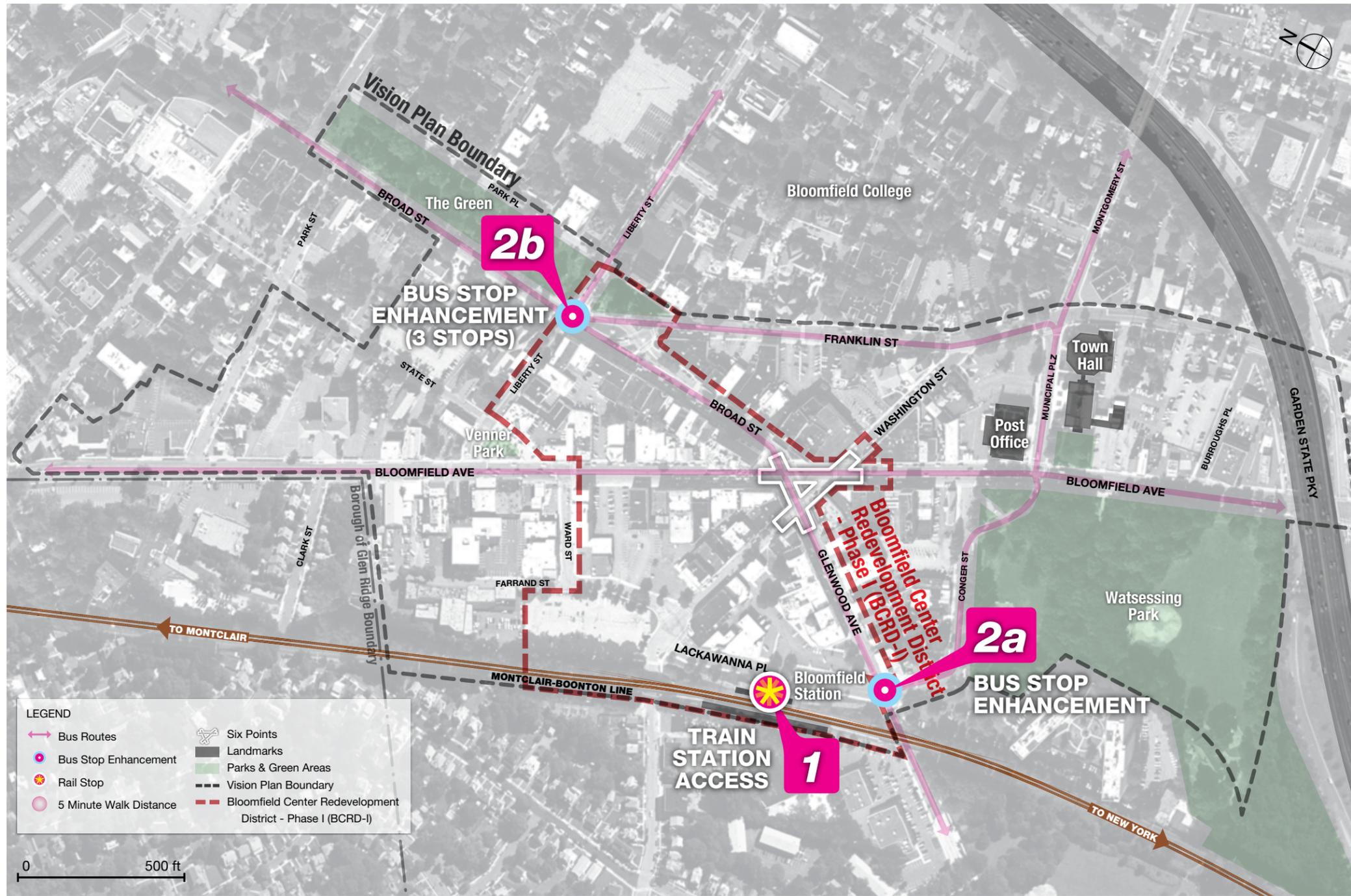


Exhibit 8. Transit Access

Refer to Schedule A for precise location of "BCRD-I" boundary

Transit Access

One of Bloomfield Center's greatest attributes is its access to public transportation alternatives. With a growing focus on sustainability and reducing dependency on personal automobiles, this Redevelopment Plan encourages that Bloomfield Center is maximizing the utilization of its existing public transportation infrastructure.

1. Bloomfield Station

Bloomfield's train station is a historic station on NJ Transit's Montclair-Boonton train line. With direct access to midtown Manhattan, Newark and Hoboken, this station should offer convenience to commuters, local residents and visitors. In order to achieve this goal, several key improvements are recommended:

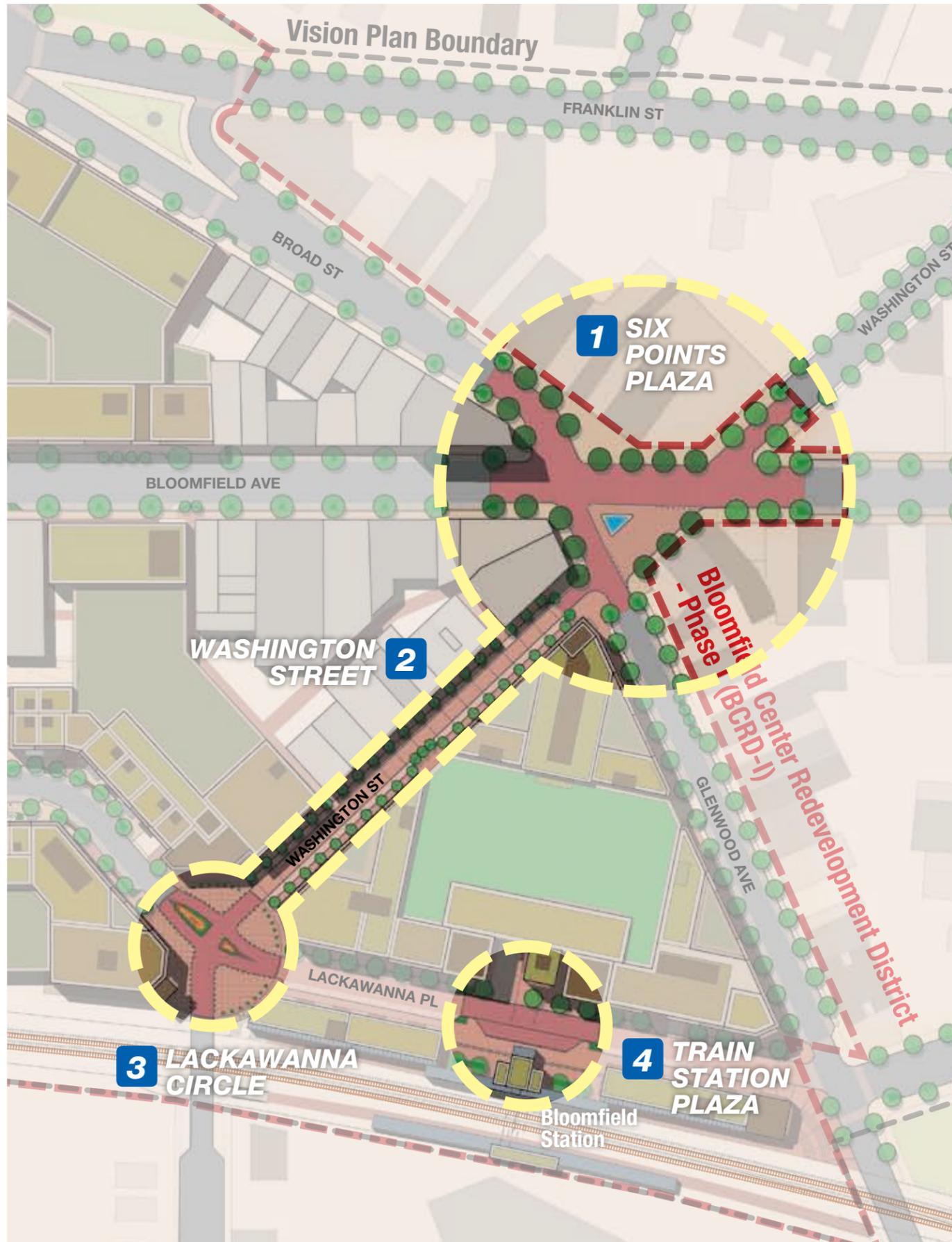
- The existing station was recently reopened and becomes the primary access to both platforms and waiting area for the train station by reopening the tunnel
- Side access points to the station, located on Washington Street and Glenwood Avenue, redesigned to be prominent features in the urban landscape-wide "Grand Stairways", integrated into the surrounding streetscape and plaza spaces
- Ample bicycle facilities at the station including bicycle racks and lockers
- Pedestrian, vehicular and bicycle wayfinding signage located throughout the downtown directing people to the station

2. Bus Stops

Bus stops located throughout the downtown provide an important means of transportation and access to other towns and cities in the region. In addition, bus stops draw large groups of users to the downtown providing a strong customer base and important source of revenue for many downtown stores. To ensure that existing bus stops are strategically located and are comfortable, safe and convenient to users, the following improvements are recommended:

- Shuttle to link remote parts of Bloomfield to the center, or examination of existing transit to ensure that remote areas are linked to the center
- A study that analyzes existing bus stop locations throughout the downtown and confirms this plan's recommendations
- All bus stops include at a minimum: seating, bus shelter, signage, bus schedule/route indication and ample lighting
- Link bus stops with pedestrian crossing and public space improvements





Public Spaces

In order to provide ample public space and help promote the development of a vibrant, community oriented downtown, below are a series of public space improvements for specific locations within the BCRD-I. The overall public space strategy will include:

- Creating an integrated traffic, circulation and open space plan
- Relating development/building plans with public space improvements
- Highly functional public spaces
- Implementing a public space management program

1. Six Points Plaza

Six Points is the historic hub of Bloomfield Center. This intersection is envisioned as a bustling pedestrian environment that acts as the “arrival point” and pedestrian gathering area for the downtown. In order to achieve this vision, several key improvements will serve to calm traffic and make this area more “pedestrian friendly”, as follows:

- The small section of Washington Street, between Bloomfield Avenue and Glenwood Avenue closed to vehicular traffic and repaved using decorative pavers in order to encourage pedestrian use and maintain the historic roadway pattern.
- The new triangle formed by the closure of this portion of road transformed into a pedestrian plaza. A decorative or sculptural feature such as a fountain or large art piece should be located at the point of this plaza, marking the center. This feature should be well lit to be clearly visible at night.
- The entire intersection of Bloomfield Avenue, Broad Street, Washington Street and Glenwood Avenue paved using decorative pavement such as concrete pavers, brick pavers or stamped/colored resin or concrete. Stamped/colored asphalt should not be considered as an option.
- Gateway elements provided at Broad Street and Washington Street entrances. Plans for these elements are currently being developed by the BCA.

2. Washington Street

Washington Street will become an ‘event street’ for the downtown and a place for town events and celebrations. During holidays the street may be closed to facilitate a winter celebration, or during the spring, it may become home to the local farmers or craft market. To this end, the following actions will ensure that the street can accommodate such events. This can be used in coordination with events currently being held at Venner Park and Broad Street.

- Installation of removable bollards so that this street can be temporarily closed to vehicular traffic during events.

- On-street parking lanes designed as “flexible space” could be used for outdoor seating or outdoor vending at the BCA or town’s discretion.
- Electrical hook-ups installed along curb lines, appropriately spaced and secured, to provide electricity to vendors, lighting or other uses as required to properly facilitate an event.
- Decorative paving installed across the entire roadway. This may be in the form of concrete pavers, brick pavers or granite cobbles. Stamped concrete, resin or asphalt should not be considered as an alternative due to durability, maintenance and aesthetic issues.

3. Lackawanna Circle

At the terminus of Washington Street and the base of the train station will be “Lackawanna Circle”. A standard element of a transit village, this plaza will act as a gathering space for commuters and residents alike. Improvements to this plaza should include:

- Ample benches, lighting and outdoor seating provided.
- Public art and sculptural elements located appropriately within this space.
- Ample, visible and convenient bicycle parking provided for commuters and visitors.
- The entire intersection paved using decorative pavement such as concrete pavers, brick pavers or stamped/colored resin or concrete. Stamped/colored asphalt should not be considered as an alternative.

4. Train Station Plaza

A new drop-off area along an improved Lackawanna Place connecting the train station to the proposed parking deck. This space is envisioned as a passive gathering space used primarily by train commuters. Improvements include:

- Ample benches, lighting and outdoor seating.
- Public art and sculptural elements located appropriately within this space.
- Ample, visible and convenient bicycle parking.
- Drop-off area and roadway at crossing using decorative pavement such as concrete pavers, brick pavers or stamped/colored resin or concrete in the area linking the train station building to the entry to the proposed parking structure. Stamped/colored asphalt should not be permitted.

Exhibit 9. Public Spaces

Refer to Schedule A for precise location of “BCRD-I” boundary

Land Use Regulations

The Land Use Regulations address permitted uses and parking. Density and other zoning regulations will be addressed throughout the Design Standards and Form Based Zoning section.

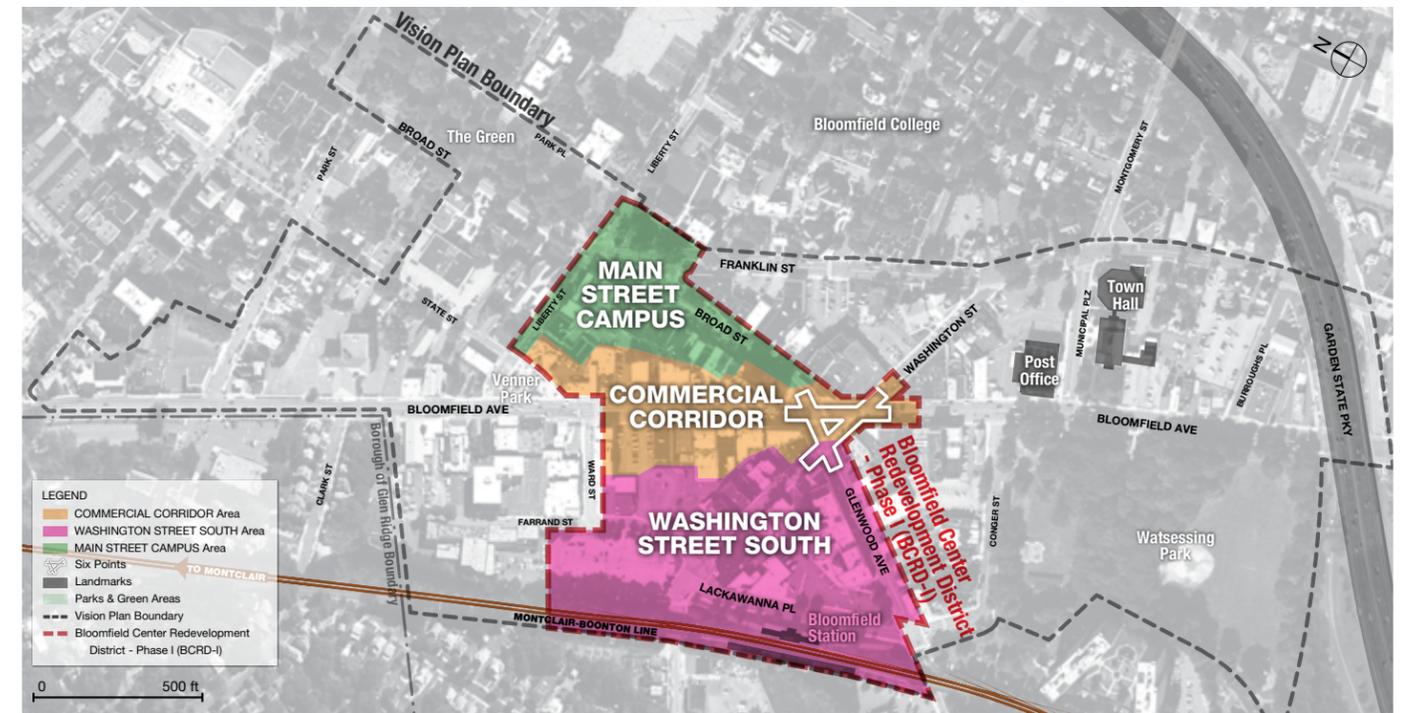
- 4.1 Permitted Uses
- 4.2 Parking Schedule

Table 1. Permitted Uses

Bloomfield Center Redevelopment Plan Permitted Uses	Commercial Corridor	Washington Street South	Main Street Campus
<i>Uses permitted by specific provision of the Municipal Land Use Law are also permitted in the Redevelopment Plan Area.</i>			
Single-use multi-unit residential buildings		✓ (1)	
Townhouses		✓ (1)	
Retail stores, service and financial establishments	✓	✓	✓
Buildings containing a mixture of residential and commercial uses except that the residential use shall be restricted to the upper floors	✓	✓	✓
Buildings containing a mixture of retail and office uses except that the office uses shall be restricted to the upper floors	✓	✓ (2)	✓
Office buildings	✓		✓
Specialty food markets (less than 30,000 SF)	✓	✓ (3)	
Hotels	✓ (4)	✓ (4)	✓ (4)
Restaurants & cafes	✓	✓	✓
Art studios and galleries		✓	✓
Performing arts theaters	✓	✓	✓
Education facilities			✓
Conference and Meeting Facilities	✓ (5)	✓ (5)	✓ (5)
Public plazas and recreation areas	✓	✓	✓
Indoor recreation facilities	✓	✓	✓
Organized open-air markets and festivals with temporary structures for retail sales and displays subject to all applicable state and local permits	✓ (6)	✓ (6)	✓ (6)

- (1) Ground floor residential is only permitted for buildings fronting on Farrand Street. All ground floor units should have private entrances with stoops on Farrand Street.
- (2) On Washington Street, office is permitted on second floor only.
- (3) Specialty food markets are not recommended on Farrand Street, Washington Street or Lackawanna Place.
- (4) Hotel rooms are not permitted on the ground floor. Street fronting ground floor is restricted to lobby and related uses only. Restaurants or cafes associated with the hotel that are street fronting must have separate street fronting entrances (in addition to any entries from inside the hotel).
- (5) Conference facilities are permitted as an accessory use to a hotel facility and shall be limited to the upper floors.
- (6) It is recommended that this use be focused on Washington Street as a priority and be coordinated with current events hosted by the BCA.

Exhibit 10. Districts Map



Refer to Schedule A for precise location of "BCRD-I" boundary

Permitted Uses

Permitted uses within the Bloomfield Center Redevelopment District - Phase I will be organized by Land Use District (Exhibit 10) and are shown in Table 1.

Commercial Corridor District

Bloomfield Avenue will remain as the traditional retail spine of the Downtown. Larger floor plates and ceiling heights are encouraged for new retail spaces in order to attract regional retailers, small specialty food markets, office or hotel uses. These larger floor plate uses will be permitted in this district, but must be designed to fit within the urban fabric. In addition, a greater mix of uses will be encouraged on the upper floors including office and residential. Due to the access and proximity to both the train and Garden State Parkway, office will be permitted as a primary use.

Washington Street South District

Due to its proximity to the train station, this district will be the focus of the Transit Village and will therefore have the greatest intensity of land use within the downtown. Washington Street is envisioned as a pedestrian-oriented street, lined with boutiques, art galleries, cafes and restaurants with outdoor seating. Farrand Street is envisioned as a residential street lined with townhouse stoops and loft style apartments above.

Main Street Campus District

The Main Street Campus District centers around Broad Street and Bloomfield College and shares many of the same permitted uses with the Washington Street South District, but will have a higher degree of "college town" uses such as higher density residential buildings, smaller scale, service based retail with some upper level residential and office (second floor only), and professional office buildings, educational and cultural uses.

Parking Schedule

Introduction

In urban and town center developments throughout New Jersey and the United States, it has become increasingly clear that parking is one of the largest challenges. The first challenge is associated with the general languishing of downtown development where buildings have given way to surface parking lots. These sites do not contribute to the principles of a pedestrian oriented downtown. So as we look to redevelop these sites for higher and better use, the only solution to parking is to build either in structures or underground. This represents the second challenge – the cost of structured parking. The third challenge is the parking requirements. For many years parking requirements have been based on suburban trends, which are very different from downtown trends. This results in excessive parking requirements and leads to the construction of large, under utilized parking sties. This adds to cost and eliminates the opportunity for more productive land-use.

These challenges are often too difficult to overcome and lead to lower quality developments. It has become increasingly clear that the most successful downtown revitalizations consider parking in a pro-active way, recognizing that the right parking approach is the joint responsibility of the private developer and the City or Town. There are a number of examples around the country and in the State of New Jersey, including Morristown and New Brunswick, where the governing body has employed the use of proactive and creative techniques to solve the parking dilemma.

In the “BCRP-I”, the techniques that may be employed include the following:

1. Shared Parking Plan
2. Downtown-wide Parking Management Plan
3. Parking Authority low-cost bond financing
4. Tax Pilot program for investment in Parking
5. Developer financial contribution for parking financing “pool” in lieu of providing parking.

The following section will describe these techniques and outline base parking requirements, and how they are adjusted utilizing shared parking formulas and financing alternatives.

Shared Parking

Shared parking occurs when multiple land uses share common parking spaces. For example, if a downtown apartment dweller typically vacates a parking space at 7:00AM every weekday morning, that same parking space can become occupied by a downtown employee, shopper or visitor during the day. This “sharing” of parking spaces maximizes parking utilization and efficiency.

When applying this principle to parking demand generated by a multi-use redevelopment, the savings can be significant.

The primary purposes of developing a base shared parking plan for the Bloomfield redevelopment are to: 1. determine the logical locations for shared parking facilities and 2. ensure that the parking supply is not overdeveloped.

Methodology

The development of a shared parking model is characterized by a number of unique elements:

1. The local “typically vacant” parking supply is recognized;
2. Parking spaces that are expected to become vacant as a result of removals or demolition are recognized;
3. Special parking factors consistent with a “downtown” as opposed to “suburban” setting are utilized;
4. Special “Percent of Peak” (POP) factors are used to adjust parking demand estimates to a specific time of day;
5. Users are expected to walk less than two blocks from their parking space to their destination.

The above elements are quantified for each potential phase of the Redevelopment Plan and formatted onto a matrix that summarizes parking supply and demand, surpluses, deficits, and “sharing” for each phase.

Analysis

In Spring 2008, the Mayor and Council commissioned parking consultant “Level G Associates” to analyze Bloomfield Center’s current parking and future “shared parking” potential based on the development impacts associated with this Redevelopment Plan.

The results of this analysis show that Bloomfield contains both CBD and Residential zones consistent with national standards, the break being Bloomfield Avenue, with the area this plan describes as the Main Street Campus district being the residential oriented area. Specifically, some key data points are:

- Peak parking in the CBD occurs around the lunch hour when 775 of the 996 (78%) municipal parking spaces were occupied.
- The area of the CBD between Bloomfield Avenue and the train station experiences the highest parking space occupancy – 553 of 668 spaces, or 83%, were occupied at the 1PM peak.
- At 11PM peak parking occurs in the Residential zone (74% parking occupancy) – at the same time the CBD has relatively little parking activity (12% parking occupancy).

These and other characteristics guide the downtown parking plan that includes public parking distributed in each development in order to support the parking needs for commuters, retailers and residents.

Parking Calculation

The following section describes a step-by-step procedure to determine the amount of parking that will be assigned to each development method. This procedure recognizes the shared parking and includes, among other things, a provision that allows developers to pay into a public parking development “Fund” in lieu of providing on site parking.

1. Determine minimum parking requirement per Individual uses - The minimum number of parking spaces that are to be provided and maintained for each use shall be determined based on following parking ratios:

Table 2. Base Parking Calculation

Residential	1.4 per Unit
Office	2.9 per 1,000 SF
Retail / Commercial	2.7 per 1,000 SF
Hotel	1.3 per Room
Restaurant	0.3 per Seat
Movie Theater	0.3 per Seat
Conference	5.0 per 1,000 SF
Place of Worship	0.2 per Seat
Other	Per APA Parking Standards

2. Adjust for shared parking - The minimum parking requirement for each use shall be multiplied by an “occupancy rate” as determined by a study of local conditions (or as found in Table 3. Occupancy Rate Calculation). Each use should be calculated for the weekday, night and evening periods, and weekend night, day and evening periods respectively.
3. Tabulate minimum parking requirement - Add up the adjusted parking requirement for each use (by time period).
4. Determine minimum parking requirement - The highest parking requirement of the six time period totals shall be the minimum parking requirement for the mixed use project.

Table 3. Occupancy Rate Calculation

	Monday-Friday			Saturday-Sunday		
	8A-6P	6P-12A	12A-8A	8A-6P	6P-12A	12A-8A
Residential	60%	100%	100%	80%	100%	100%
Office	100%	20%	5%	5%	5%	5%
Retail / Commercial	90%	80%	5%	100%	70%	5%
Hotel	70%	100%	100%	70%	100%	100%
Restaurant	70%	100%	10%	70%	100%	20%
Movie Theater	40%	80%	10%	80%	100%	10%
Conference / Conven.	100%	100%	5%	100%	100%	5%
Place of Worship	10%	5%	5%	100%	50%	5%
Other	Per ITE, APA or applicant’s study (subject to municipal approval)					

Source: Victoria Transport Policy Institute

The above table indicates the percent adjustment of the minimum parking requirement during each time period for shared parking. Percentages set forth in the table are set to include a small “safety margin” of parking beyond that minimally needed to serve an average peak demand.

Parking Provisions

1. 100% of the parking supply shall be provided within 400 feet of an entrance to the proposed building(s) it will serve unless waived via terms of item (2) and / or (3), of the “Parking Provisions”.
2. Other parking spaces in the vicinity of the project may be used to satisfy portions of the minimum parking requirement if the applicant can secure such parking through lease or other similar terms; or if it can be demonstrated through study that certain public parking areas are typically vacant during the peak demand period of the project or will become vacant as a result of removals or demolition, all subject to the approval of the Mayor and Council.
3. The developer’s parking assessment will be vetted with this parking plan to develop a final parking configuration. If the developer cannot provide 100% of the required parking on-site, as a part of the Redeveloper Agreement, a determination will be made on how the developer will contribute into a pooled fund for its parking requirement. The amount of the contribution will be negotiated as a part of the Redeveloper Agreement. This fee will be deposited and utilized for the development, improvement or maintenance of public shared parking in the “BCRD-I”.





Bloomfield Center Redevelopment

This is the primary section for guiding appropriate size, density, form and aesthetics for each new development. This section has been designed as both an educational tool and prescriptive “zoning code”. This Form Based Zoning encourages flexibility while ensuring that the vision and goals set forth by the community and Mayor and Council are met.

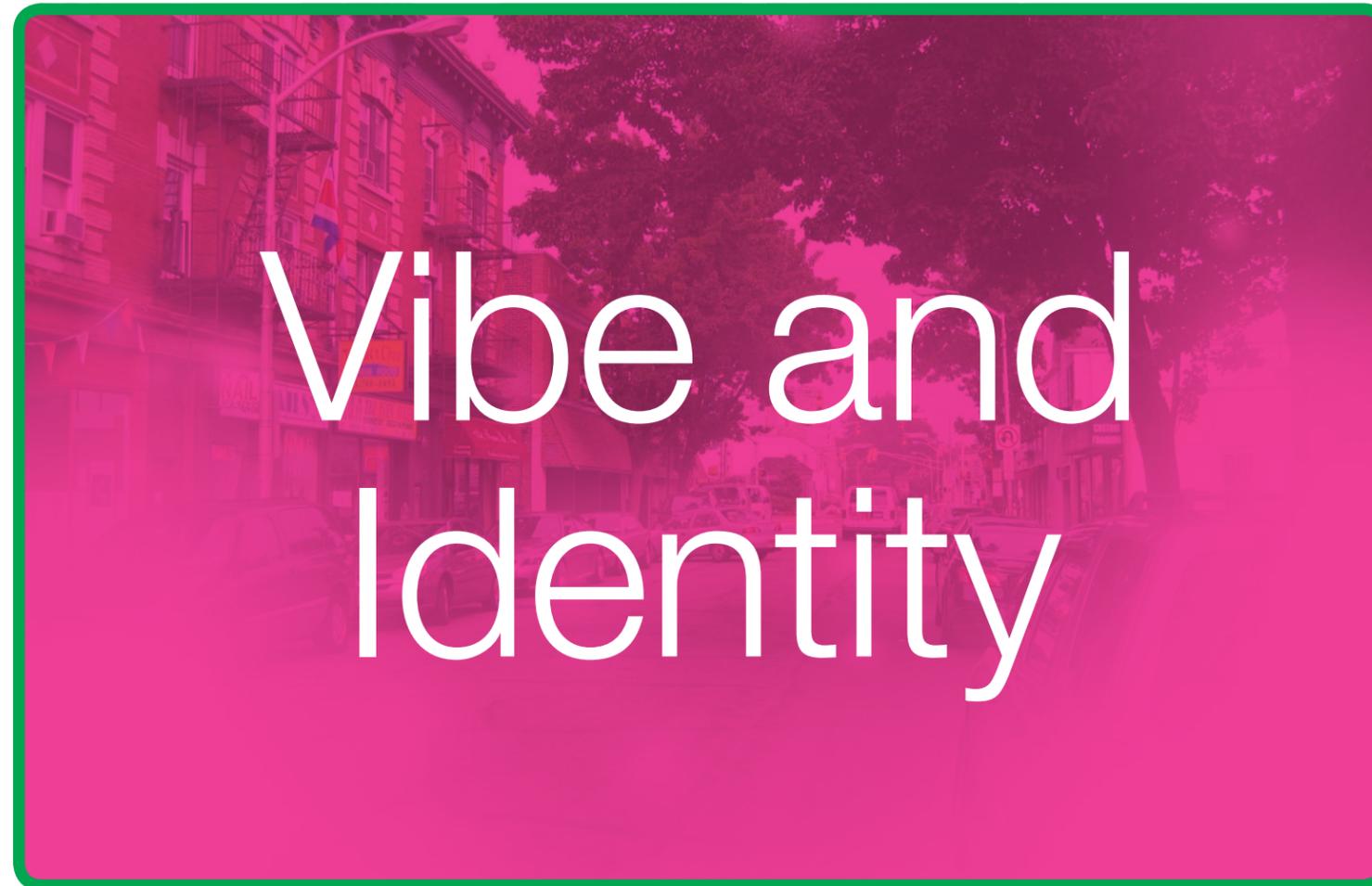
This section is comprised of both “Guidelines” and “Requirements” as deemed appropriate. The intent is as follows:

Guidelines - Guidelines are intended to be “suggestive” items that each development should strive to achieve in order to contribute to the overall goals of this plan.

Requirements - Requirements are intended to be “prescriptive” and are required to be met in order to be in compliance with this plan.

- 5A. Vibe and Identity
- 5B. The Building Form
- 5C. Preservation & Rehabilitation of Buildings
- 5D. Parking & Site Services
- 5E. A Comfortable Environment for Pedestrians

Design Standards & Form Based Zoning



- 5A.1 Place-Making Districts
- 5A.2 Commercial Corridor
- 5A.3 Washington Street South
- 5A.4 Main Street Campus

*Design
Standards
& Form
Based
Zoning*

5

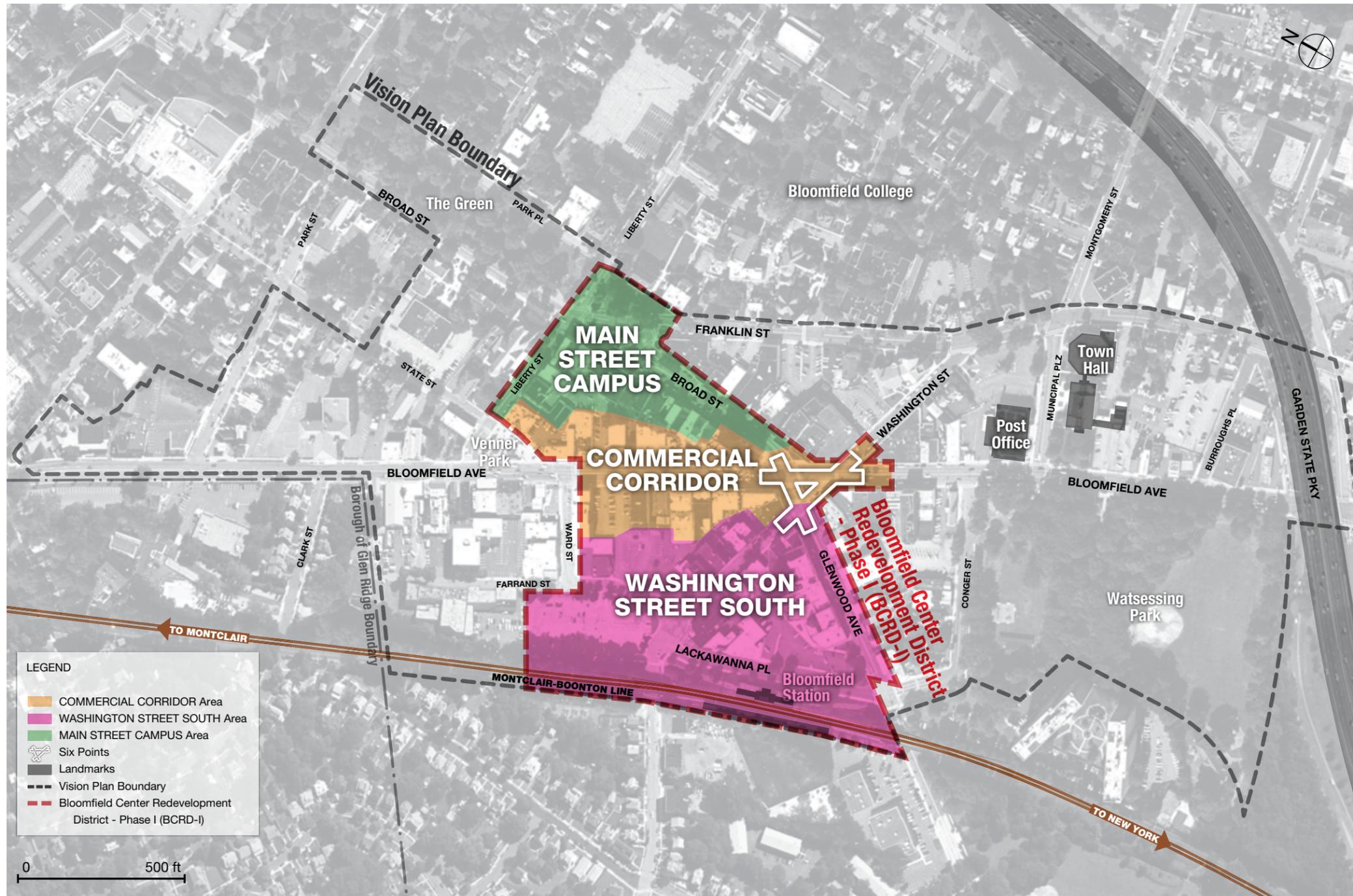


Exhibit 11. Place-Making Districts Map

Refer to Schedule A for precise location of "BCRD-I" boundary

Place-Making Districts

Overview

Three of the four "Place Making Districts" identified in The Vision Plan are located with the BCRD-I. Each of these districts offers a unique experience and "vibe" for visitors while maintaining a unified, seamless feel through the use of common streetscape elements. These "Place Making Districts" form a basis for design direction and massing and are influenced by their existing physical, spatial and experiential qualities. This Redevelopment Plan encompasses:

- Commercial Corridor
- Washington Street South
- Main Street Campus

Guidelines

- All developments should reflect the vibe and identity of the "place making district" that they are located within as shown on Exhibit 11 and as described in the following pages.
- In all districts, locating appropriate elements of sustainable design, such as use of local and sustainable materials, daylighting, passive solar, and native and place appropriate plantings, should be incorporated.



Landscaped Plazas



Signage & Wayfinding System



Pedestrian & Transit Friendly Plazas



Urban Scale Retail



Architectural Variation



Mixed-use Buildings



Pocket Parks

Commercial Corridor

Vibe

“Bloomfield Avenue is an exciting, busy corridor that offers excellent opportunity as a regional shopping hub and office destination.”

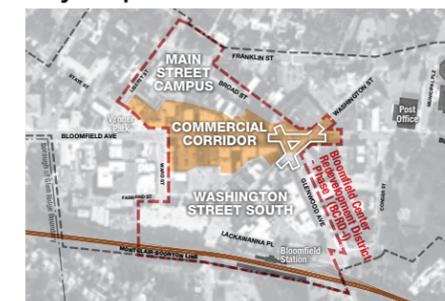
Identity

- Urban Scale design appropriate for a main thoroughfare experience
- Mixed-use Programming
- Regional scale commercial space accommodations
- Eclectic mix of architectural styles with:
 - Respect for the character of existing historic buildings
 - Contemporary designs for new structures
 - Sophisticated, well thought out detailing and systems
- New construction has emphasis on modern materials such as:
 - Glass
 - Metal
 - Concrete
 - Panel systems

Guidelines

- All developments should reflect the vibe and identity of the “place making district” that they are located within as shown on Exhibit 11 and as described in the following pages.
- In all districts, locating appropriate elements of sustainable design, such as use of local and sustainable materials, daylighting, passive solar, and native and place appropriate plantings, should be incorporated.

Key Map





Adaptive Re-use and Infill



Green Streets



Row House Character



Mixing of Materials



Historic Train Station



Pedestrian Scale Shops



"Funky" Design Elements

Washington Street South

Vibe

“Centered around Washington Street, the new boutique/event street, this district is envisioned to be a fun and funky district with an urban feel – anchored to the industrial history and rail lines.”

Identity

- Pedestrian scale design appropriate to an urban “event street”
- Commuter friendly lifestyle
- Interaction between building & street
- Use of plants and vegetation
- Made up of Architectural styles including:
 - Existing Historic Warehouse character
 - Modern Industrial aesthetic
 - Loft-style & Row House living
 - Mixing of contemporary and traditional detailing
 - Funky “moments”/Elements
- Emphasis on mixing of materials including:
 - Metal
 - Brick
 - Salvaged materials
 - Wood details
 - Green (vegetation)

Guidelines

- All developments should reflect the vibe and identity of the “place making district” that they are located within as shown on Exhibit 11 and as described in the following pages.
- In all districts, locating appropriate elements of sustainable design, such as use of local and sustainable materials, daylighting, passive solar, and native and place appropriate plantings, should be incorporated.

Key Map





Traditional Campus Environment



Contemporary Interpretation of Traditional Design



Traditional Materials, Contemporary Details



Variety of Architectural Elements



Landscaped Gathering Spaces



Historic Elements



Daily Shopping "Main Street" Character

Main Street Campus

Vibe

"The traditional town green and college campus set the tone for this district and provide a passive, lush family atmosphere and community gathering space."

Identity

- A traditional "Main Street" atmosphere and scale
- The town's cultural center
- Place for daily shopping
- Lush landscape elements
- Architecture is made up of:
 - Victorian style existing buildings
 - Contextually sensitive new buildings
 - The use of traditional architectural elements
- Emphasis on traditional materials including:
 - Brick
 - Stone
 - Wood (or its equivalent such as fiber cement board)

Guidelines

- All developments should reflect the vibe and identity of the "place making district" that they are located within as shown on Exhibit 11 and as described in the following pages.
- In all districts, locating appropriate elements of sustainable design, such as use of local and sustainable materials, daylighting, passive solar, and native and place appropriate plantings, should be incorporated.

Key Map





- 5B.1 Overview
- 5B.2 Urban Context
- 5B.3 The Building Envelope
- 5B.4 Building Articulation
- 5B.5 Architectural Elements
- 5B.6 Storefront and Signage

*Design
Standards
& Form
Based
Zoning*

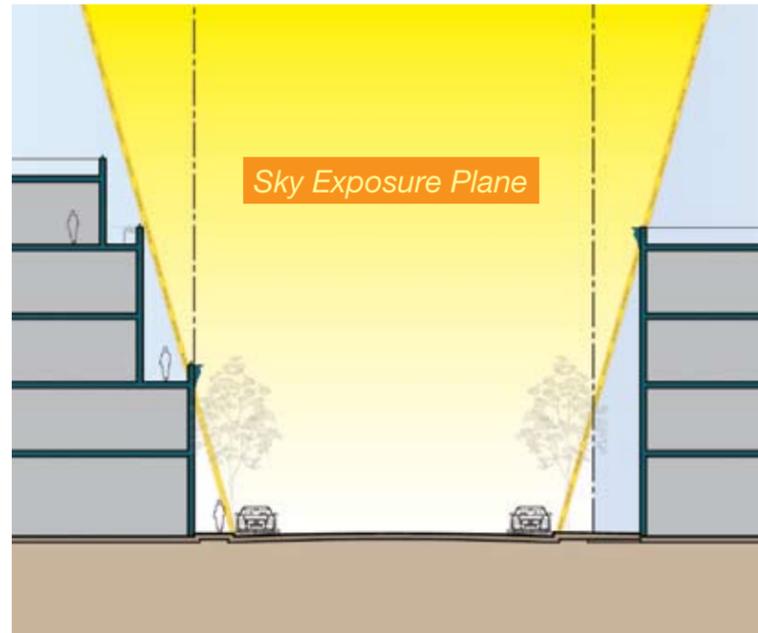
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The Building Form Overview

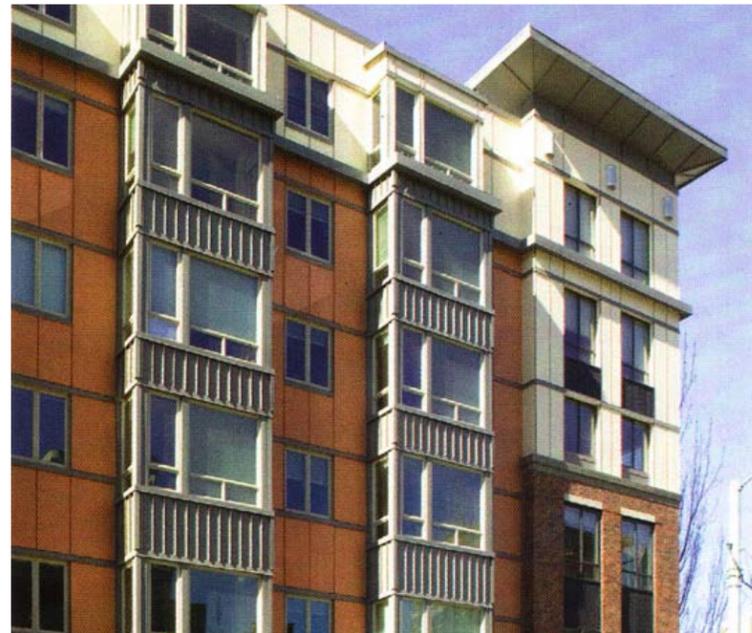
Introduction

The form of the various buildings that comprise up a downtown are key in defining a great urban environment. This Redevelopment Plan strives to allow for appropriate density while “breaking down” the mass of new buildings so they do not impose on the pedestrian environment. Through various techniques, this section strives to guide redevelopers and architects in creating buildings that are the appropriate scale and character while not limiting creativity and architectural expression. These guidelines outline the following elements of Building Form:

- Urban Context
- The Building Envelope
- Building Articulation
- Architectural Elements
- Storefronts and Signage



The Building Envelope



Building Articulation



Architectural Elements



Storefronts and Signage

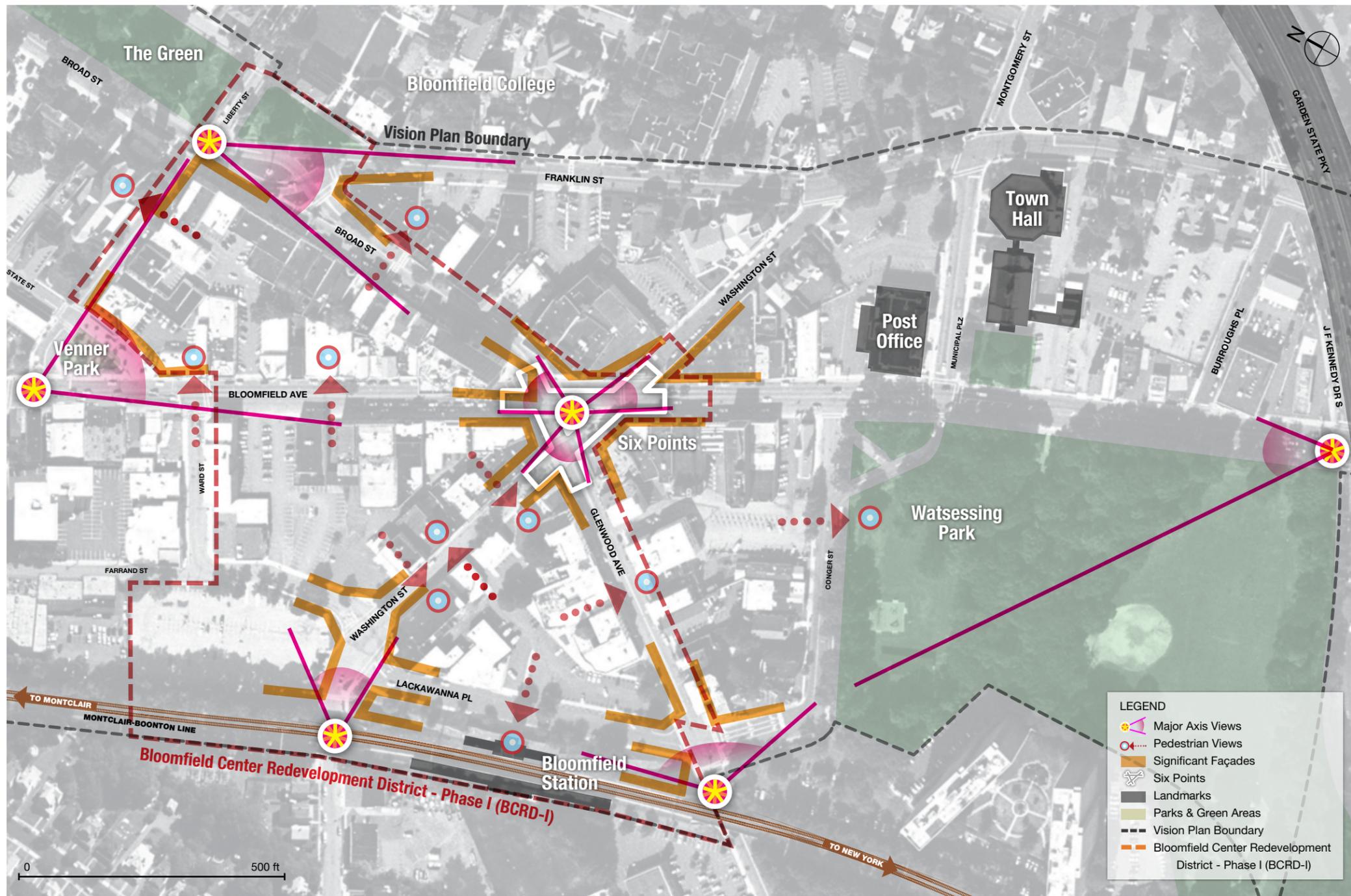


Exhibit 12. Urban Context Plan

Refer to Schedule A for precise location of "BCRD-I" boundary

Urban Context

Overview

Bloomfield Center's street layout and urban form create interesting buildings and public spaces integral to a vital downtown environment. Approaching downtown from various directions, visitors are offered views 'framed' by the street wall and terminated with either a building or area of open space. Taller towers can provide orientation while the open spaces and public plazas can create a sense of 'relief' from the consistent street wall. Terminating or framing views with architectural elements that have strong connection to the streetscape and public spaces will create interesting and dynamic buildings and experiences.

Guidelines

- Buildings that terminate key views, such as "gateways" into town, should respond accordingly with a significant architectural element or intervention (i.e. corner tower, corner bay plaza space etc).
- Buildings and edges that frame key views or public spaces should be designed and planned to create an interesting "street wall". This may be accomplished through series of design moves as outlined in the "Building Articulation" section of this plan.
- Minor view corridors, such as those that terminate pedestrian alleys, parking exits and other such points, should respond with architectural and urban design interventions of a lesser scale than those of key views (i.e. public space, sculpture element etc).

The first step in determining a planning and design direction for a development project is to evaluate the site and the surrounding context. The urban context plan (Exhibit 12) establishes some of the key considerations for planning a building or development site.





Varying building height and rooflines to create a rhythm



Changes in scale



Building height and roofline variation

The Building Envelope

Overview

In order to ensure that the “mass” of a building is kept in context with streets and surroundings, minimum and maximum heights and a “sky exposure plane” are established. The “sky exposure plane” is an invisible “vertical setback line” projected up from the curb line at a predetermined angle. Buildings can only “pierce” the sky exposure plane in limited ways forcing them to either set the building back from the street or gradually “stepback” their upper floors to allow for ample light and air to reach the street.

The Height Limits and Sky Exposure Plane (“SEP”) are determined by the width and character of the streets on which they front. In the “BCRD-1”, the streets have been divided into four zones, each with unique guidelines (Exhibit 13. Building Form Zones). The building must comply with the Height and Sky Exposure Plane requirements set forth for the street or streets on which it fronts.

Building Height and Transition

This plan encourages the voluntary assembly of small properties in order to maximize development potential. To facilitate this, at the discretion of the Mayor and Council, property owners that assemble parcels to create larger development sites may be granted permission to:

- Extend their building to the Bonus Height
- Pierce the SEP

The Building Heights, Bonus Heights and SEP are also subject to evaluation based on their impact to adjacent properties and the streets on which they front. Building heights should transition from adjacent uses and buildings as to not negatively impact these buildings and allow air and light to the street.

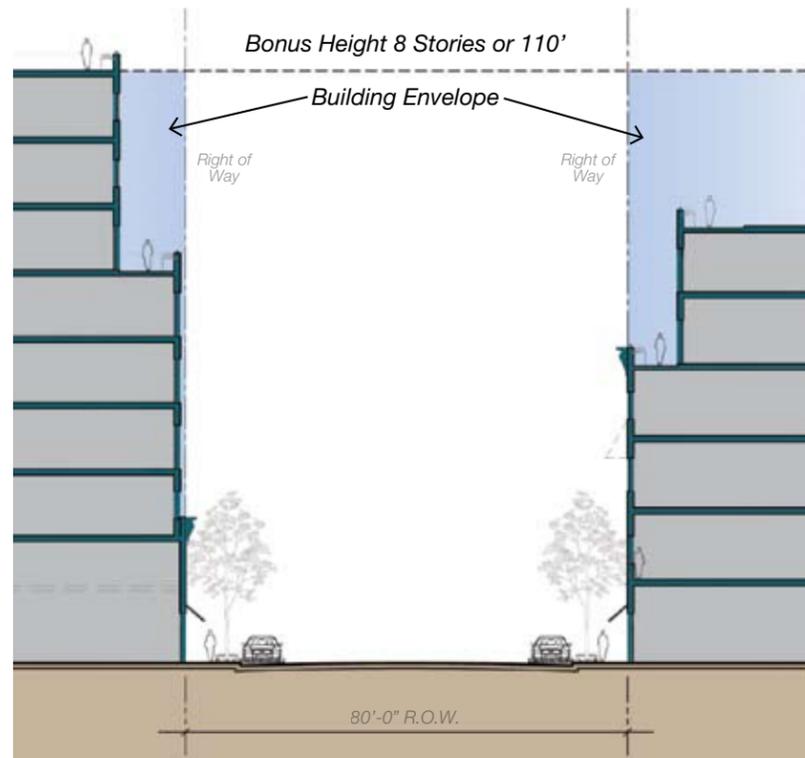
Methodology for Determining Bonus Heights

The combination of several characteristics will help determine if a property may be allowed to take advantage of the Bonus Height or pierce the SEP. Each of these characteristics provides an incentive to the property owner while working towards meeting the goals and vision of the community. These methods are intended to be reviewed and applied together when analyzing a request to extend into the Bonus Height.

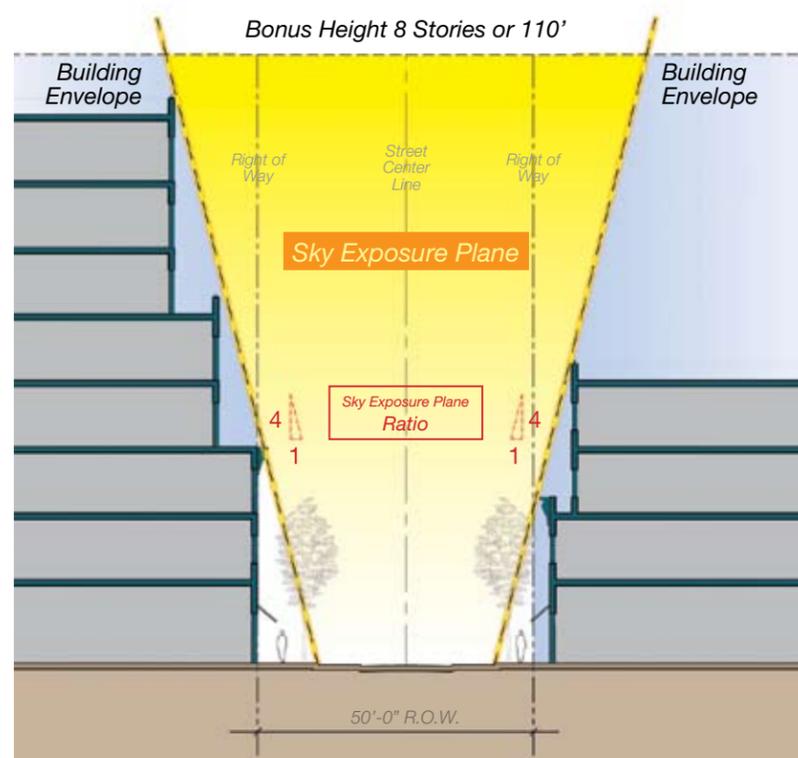
- Affordable Housing – Often times the bonus height will be utilized to allow an increase in residential density on the site. In this case, the town should encourage the developer to use a portion of this additional density to contribute to the Affordable Housing requirements of the town.
- Parcel Size – This plan encourages the assemblage of smaller parcels in order to create larger, more efficient development sites. To facilitate this, property owners with larger or assembled parcels should be permitted to pierce the SEP or extend to a bonus height based on the relative size of the parcel.

- Percentage of Block – A variety in heights and rooflines is a traditional characteristic of Bloomfield Center. In order to maintain this variety, only a portion or percentage of a block should be permitted to extend to the full available Bonus Height. The remainder of the block should remain within the permissible heights and SEP as shown.
- Transitions and Adjacency – It is important to allow for proper height transition between existing buildings and proposed development. This is most important when adjacent to a historic building or a building that is likely to remain. Building heights should be reviewed in comparison to these adjacent properties and heights should transition properly from the “mean height” of adjacent properties to the proposed height of the new building(s).

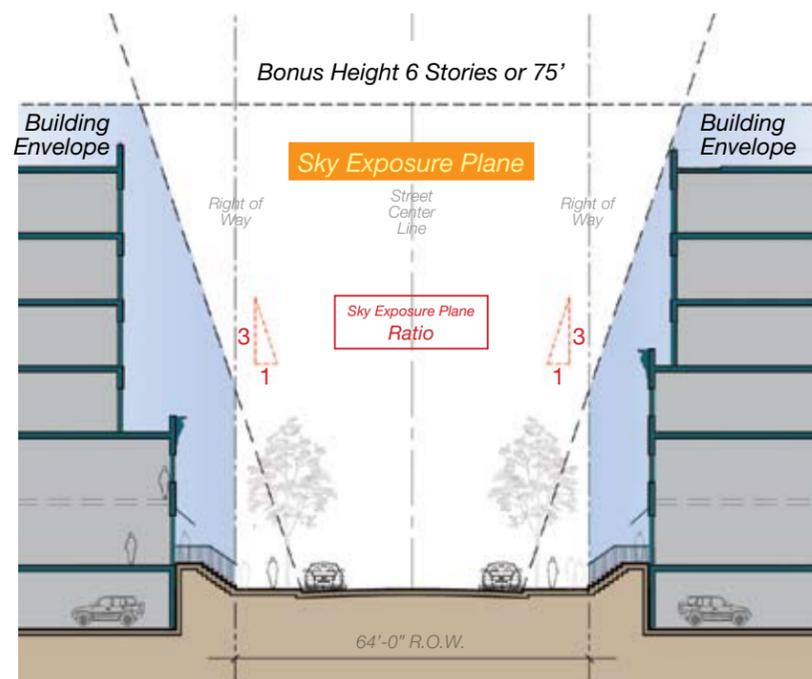
STREET TYPE 1 - Bloomfield Avenue



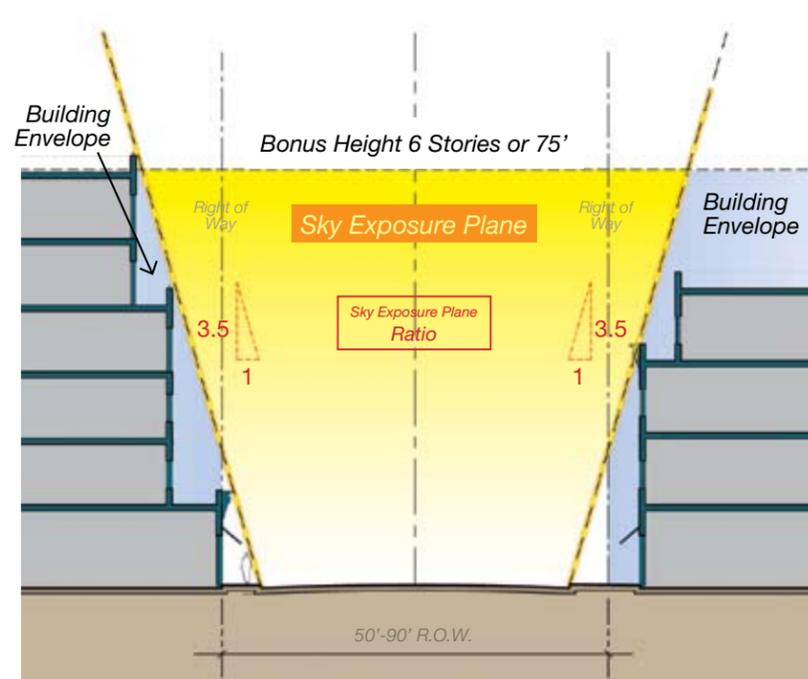
STREET TYPE 2 - Washington Street South, Lackawanna Place, Glenwood Avenue



STREET TYPE 3 - Farrand Street, Ward Street



STREET TYPE 4 - Broad Street, Liberty Street



The Building Envelope

(continued)

Street Type 1 Requirements

- Bonus Height: 8 Stories or 110'
- Maximum Height: 6 Stories or 88'
- Minimum Height: 4 Stories or 48'
- Sky Exposure Plane: None
- Miscellaneous: One 4'-6' Building step back between the 4 and 6 stories

Street Type 2 Requirements

- Bonus Height: 8 Stories or 110'
- Maximum Height: 6 Stories or 88'
- Minimum Height: 4 Stories or 48**
- Sky Exposure Plane: 1:4

** Except on the existing train station site (Block 220 Lot 40) where a minimum of 2 stories and a maximum of 4 stories will be permitted.*

Street Type 3 Requirements

- Bonus Height: 6 Stories or 75'
- Maximum Height: 5 Stories or 56'
- Minimum Height: 4 Stories or 48'
- Sky Exposure Plane: 1:3

Street Type 4 Requirements

- Bonus Height: 6 Stories or 75'
- Maximum Height: 4 Stories or 48'
- Minimum Height: 2 Stories or 26'
- Sky Exposure Plane: 1:3.5

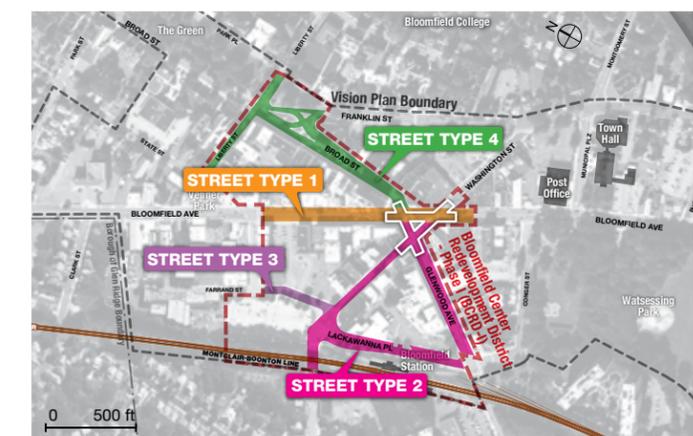


Exhibit 13. Building Form Zones

Refer to Schedule A for precise location of "BCRD-1" boundary

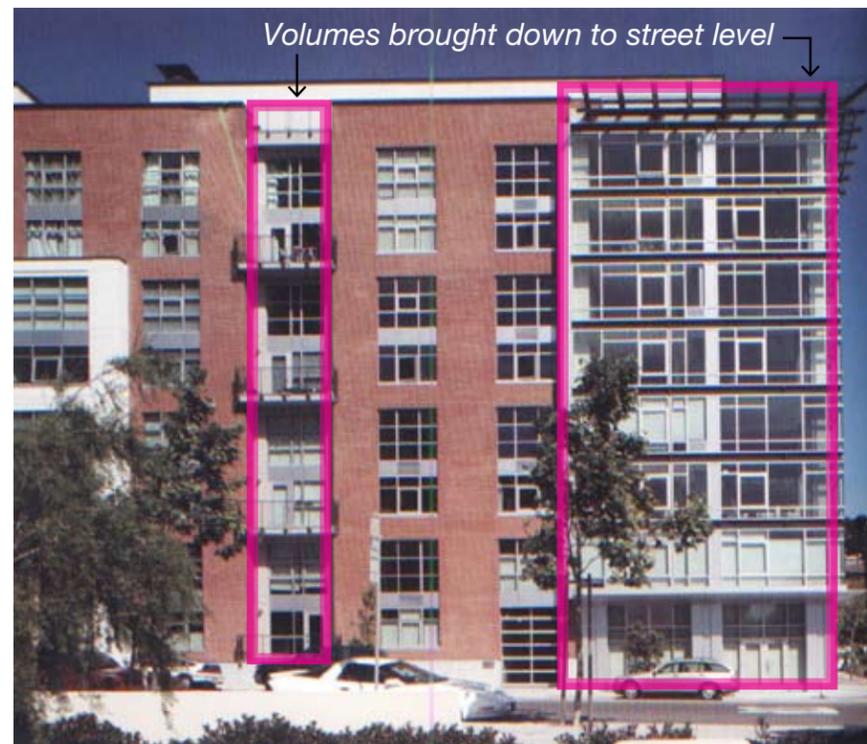
The Building Envelope

5B.3

1. Base-Middle-Top



2. Articulation of Form at the Ground Level



Building Articulation

Overview

Building Articulation is the manipulation of a building's 'forms and surfaces' in order to 1. Provide interesting and varied designs and 2. Achieve an appropriate scale. This can be achieved through a variety of methods including:

1. Base-Middle-Top Articulation
2. Building Base Articulation
3. Setbacks and Step Backs
4. Building Façade Shifts
5. Roofs & Roofscapes

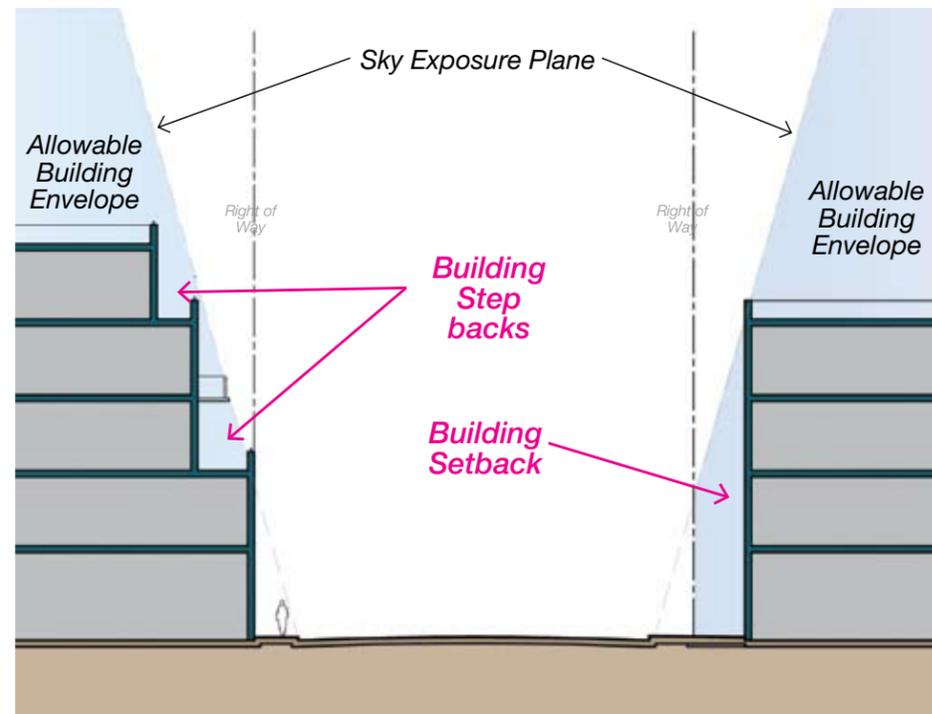
Guidelines

Development projects should demonstrate a clear strategy and intent for the building articulation using one or a combination of the following strategies:

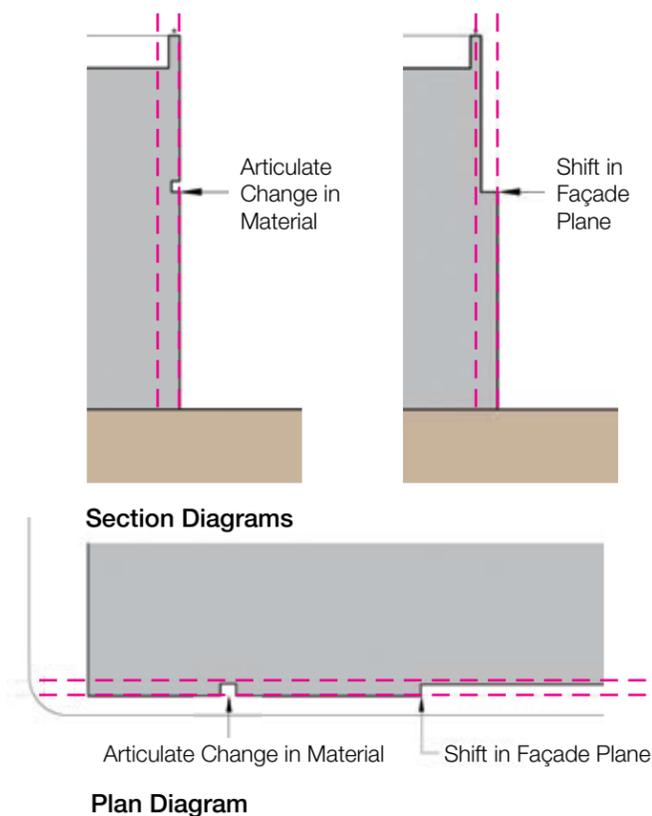
1. **Base-Middle-Top**
This traditional method for articulating a building, often appropriate for smaller scale buildings, highlights the functional and spatial distinction between the:
 - A. **Base:** The ground floor or area of the building that meets the ground. Functionally, this is often expressed by the use of more durable materials. It is also the portion of the building that is most visible, requiring adequate attention to details.
 - B. **Middle:** Typically the main bulk of the building. This area establishes the general character of the building. It typically includes repetitive elements thus requiring special attention to the organization of those elements including rhythm or pattern, depth, palette and surface quality.
 - C. **Top:** Whether articulated by the upper floors or a cornice or capping element, this area serves both a visual and functional purpose
2. **Articulation of Form through the Building Base**
This technique, often appropriate for larger buildings, provides meaningful articulation at the base of buildings providing a richer design at the street level and can be applied in a variety of ways including:
 - A. The continuation of articulated volumes and/or material changes down through the base or ground floor from the upper floor
 - B. Unique articulation of form and/or materials at the building base



3. Setbacks and Stepbacks



4. Building Façade Shifts



Building Articulation

(continued)

3. Setbacks and Stepbacks

This technique provides meaningful articulation of the building's envelope and can be used to achieve the required Sky Exposure Plane described in the previous section.

Great urban spaces and streets are not defined by the architecture of a great building but rather by the cumulative impact of many buildings lining a street creating an interesting "streetwall". When siting a building and considering "setting it back" or "stepping it back" from the right-of-way line, consideration must be given to the overall streetwall. A building "Set Back" refers to all or a portion of the base of the building being removed a certain distance from the property line. This method allows for additional space in the front yard of the building. A building "Step Back" refers to stepping back the upper floors of the building. This method provides terracing and balconies for the upper floors and permits a greater degree of natural daylighting to lower floors and the street.

Setback Requirements

For streets that have residential ground floors, a setback should be a **minimum of 5'** and a **maximum of 12'** as measured from the ROW line to the principle building façade to accommodate the stoop and front yard green space. The area created by the setback should feature a mixture of decorative pavement and appropriate landscape materials.

For all other streets, setback should be a minimum of 0' and a maximum of 5' as measured from the property line to the face of the principle building. The area created by the setback should be treated as an extension of the sidewalk and should be paved using similar materials to those of the adjacent sidewalk. This space may be used for outdoor seating or benches. In no case should this space be entirely landscaped, fenced in or in any way physically separated from the adjacent sidewalk nor should it be designed in a way that negatively impacts the existing "street wall". In addition, a building may setback up to 8' from the property line at lobby entrances to create a defined "entry space" for offices and residential units located above. This additional setback should be limited to the lobby area as defined by the building architecture.

4. Building Façade Shifts

This technique provides a meaningful, but subtle articulation of the building's envelope and can be expressed in the following ways:

- A. A horizontal, vertical or other shift of the Façade plane
- B. A horizontal, vertical or other detailed change in material, finish or pattern. The detailed change should be expressed with an accent material, reveal or other similar technique.

**Building
Articulation**

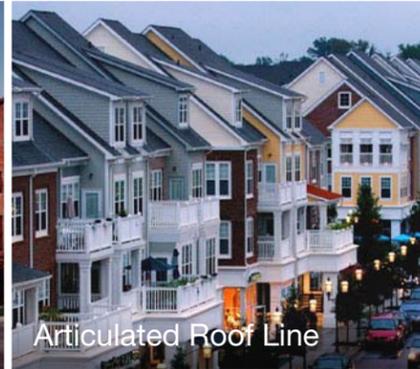
5B.4

5. Roofs & Roofscapes

Roof Articulation



Articulated Roof Line



Articulated Roof Line



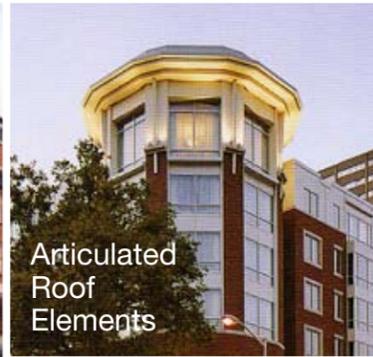
Articulated Roof Line



Articulated Roof Line



Articulated Roof Line



Articulated Roof Elements



Articulated Roof Elements



Articulated Roof Elements

Flat Roof Strategies



Cool Roof



Vegetative Roof



On Site Alternative Energy Production (Wind)



Voltaic Panel



On Site Energy Production (Solar)



Vegetative Roof

Building Articulation

(continued)

5. Roofs & Roofscapes

The building's roof is an important component of the design as well as the environmental goals of the Redevelopment Area.

Rooflines should provide variation and interest. When determining the roof type and building height, consideration should be taken regarding adjacent buildings and how the height and roof type impact the overall "composition" of the street and block. Variation in roof heights and types is encouraged. Long stretches of a consistent roof height or type should be avoided in order to avoid a monotonous roofscape.

Roofs should be flat or articulated as follows:

1. **Flat Roofs** (less than pitch 2:12)
 - Provide a basic level of cohesiveness in the overall design and an opportunity for positive environmental impact.
 - Flat roofs should have one or a combination of the following treatments:
 - "Cool Roof" having a Solar Reflectance Index equal to or greater than 78 for a minimum of 75% of the roof surface (LEED-NC Sustainable Sites Credit 7.2).
 - A vegetated roof for at least 50% of the roof area (LEED-NC SS Credit 7.2).
 - On-Site Alternative Energy Production (LEED-NC Energy & Atmosphere Credit 2).
2. **Visibly Articulated Roof**
 - This option is available when the roof design is an integral part of the overall design concept and a flat roof is incompatible.
 - Articulated roofs should also conform to the "Cool Roof" guidelines and incorporate on-site Alternative Energy Production, i.e., building-integrated photovoltaics, or solar electric panels, and wind power devices as appropriate.

Building Elements



Example of Bloomfield Center Architecture Composition



Architectural Elements

Overview

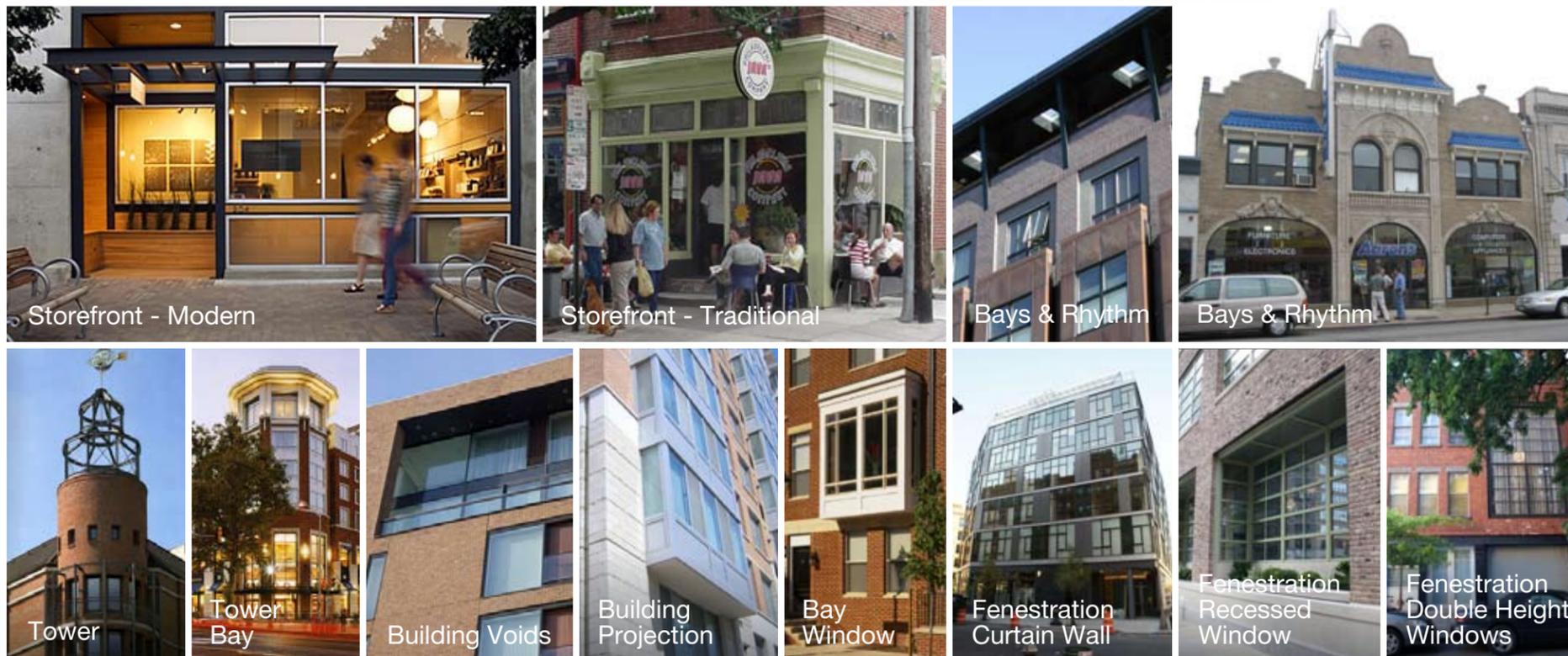
The design of a building truly emerges through the expression and detailing of the architectural “elements”. Architectural elements are the various components that make up a building, such as doors, windows, stoops and dormers. Particular building types, whether residential, retail or commercial, contain specific building elements that relate to that use, such as storefronts for retail or stoops for residential. Through high-quality design and detailing, this plan encourages an eclectic mix of architectural styles ranging from traditional to modern.

Guidelines

All proposed developments should demonstrate the following:

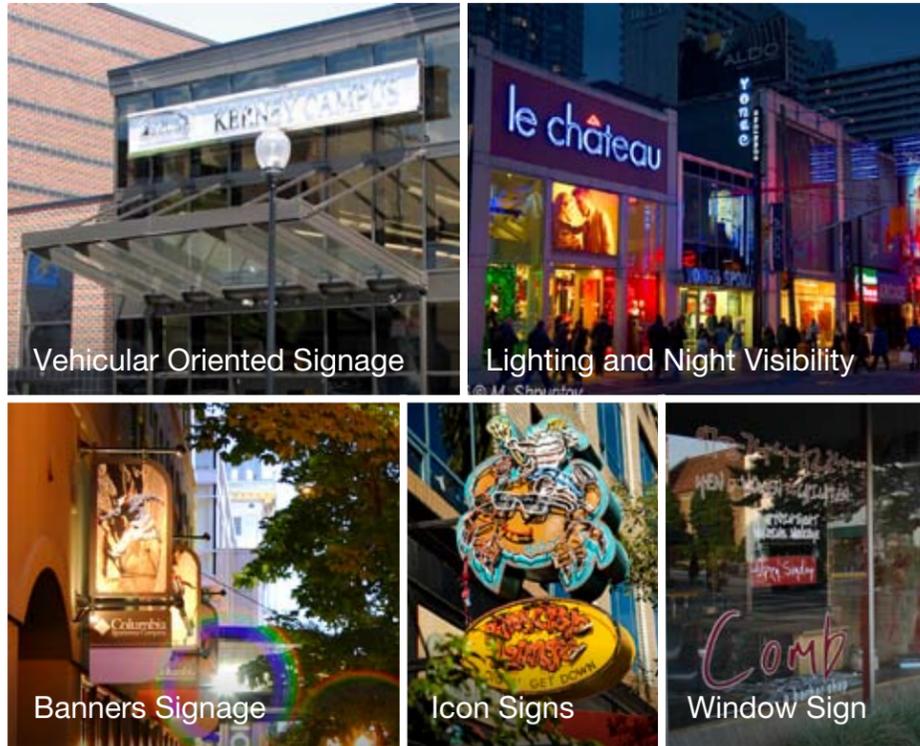
- A clear design direction
- Attention to construction quality and detail
- Attention to the use of and composition of the various Architectural Elements that comprise all building elevations

The Building Elements image on this page identifies the typical building elements. Variations of these elements as well as additional “Architectural Elements” are encouraged (as shown) and should be well designed and detailed.



Architectural Elements
5B.5

Commercial Corridor



Vehicular Oriented Signage

Lighting and Night Visibility

Banners Signage

Icon Signs

Window Sign

Main Street Campus



Sidewalk Board Sign

Traditional Material - Wood

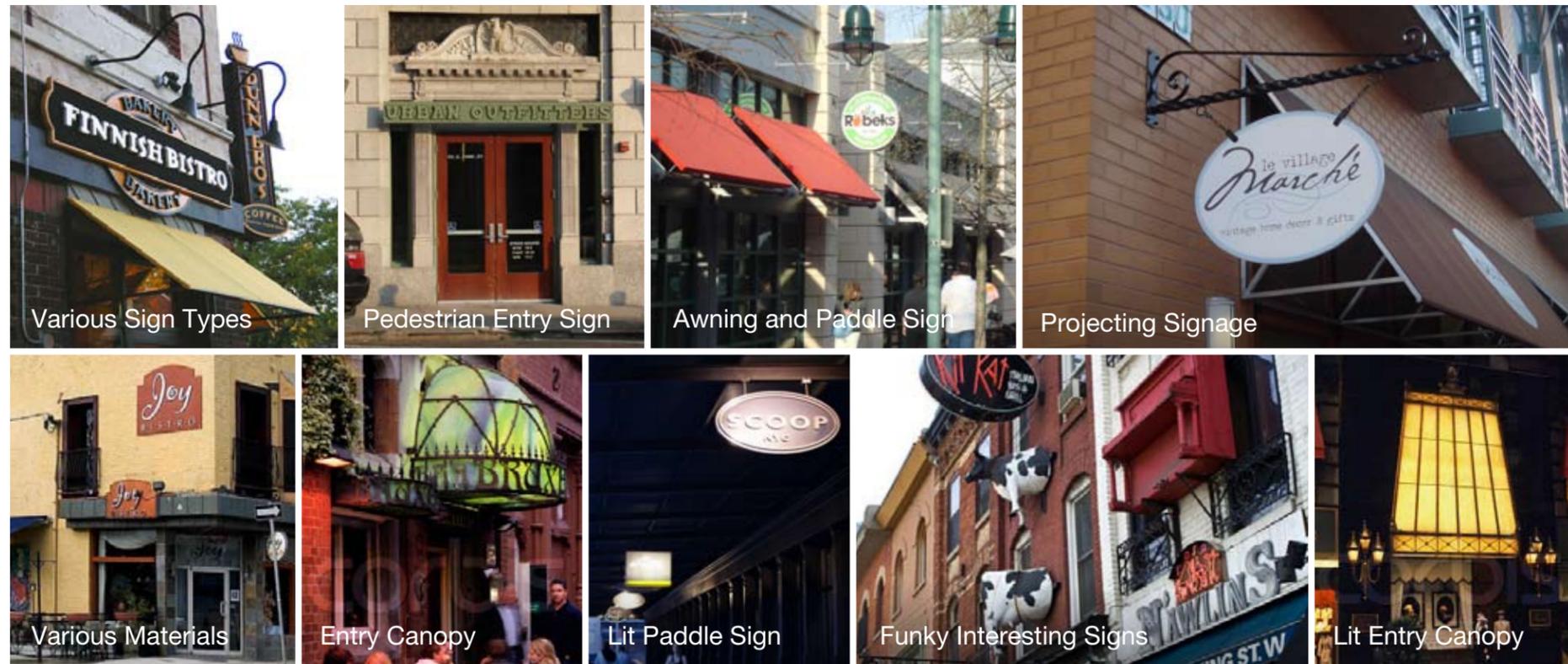
Awning Sign

Traditional Material - Brick

Goose Neck Lighting

Building Mounted Sign

Washington Street South



Various Sign Types

Pedestrian Entry Sign

Awning and Paddle Sign

Projecting Signage

Various Materials

Entry Canopy

Lit Paddle Sign

Funky Interesting Signs

Lit Entry Canopy

Storefront and Signage

Overview

A combination of types and styles of building signage in coordination with well-designed, high-quality storefronts create an enjoyable and interesting pedestrian and shopping experience. The eclectic architectural styles in Bloomfield Center lend themselves well to the incorporation of creative, innovative and unique storefronts and signage.

Requirements

All signage within the "BCRD-I" shall comply with the Bloomfield Zoning Ordinance 315-42 Signs 'A. General Provisions' and 'B. Signs Permitted in Business Zones'.

Guidelines

In addition to the requirements set forth in the Zoning Ordinance, building storefronts and signs should reflect the character of their respective "Place Making Zone" and demonstrate the following characteristics:

General

- Use of high-quality and durable materials
- Innovative Design
- Attention to Detail
- Use of durable materials
- Contribute to the overall interest and aesthetic of Bloomfield Center
- Encourage banners and projecting signs to add visual interest
- Conserve electricity through use of solar-powered and Energy Star/comparable lighting devices (LEDs, plasma, fluorescent) and time/proximity controls.
- Reduce light pollution through proper aiming and shielding

Commercial Corridor

- Oriented towards vehicles
- Larger signs
- Increased lighting and night visibility
- Avoid traditional "Goose Neck" lighting

Washington Street South

- Pedestrian Oriented signage
- Use of pedestrian scale paddle signs
- Funky, interesting sign designs
- Unique materials selection
- Unique lighting options such as Halo Lighting etc. and traditional "Goose Neck" lighting

Main Street Campus

- Pedestrian Oriented Signage
- Traditional Style and Materials
- Use of wood and external lighting (including "Goose Neck" lighting)

Storefront and Signage

5B.6



Preservation & Rehabilitation of Buildings

- 5C.1 Overview
- 5C.2 Sides and Backs of Existing Buildings

*Design
Standards
& Form
Based
Zoning*

5

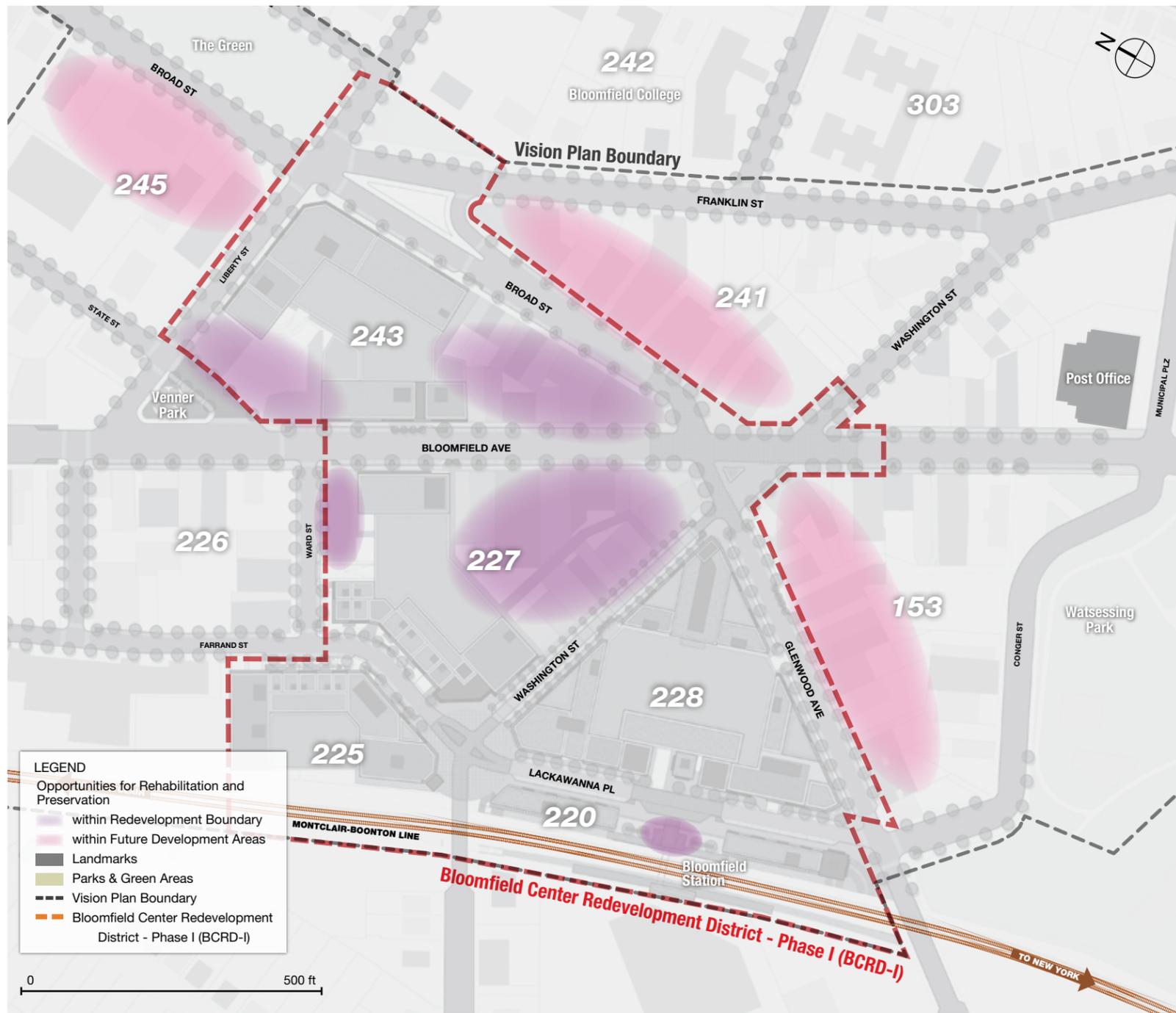


Exhibit 14. Building Preservation and Rehabilitation Map

Refer to Schedule A for precise location of "BCRD-I" boundary

Preservation & Rehabilitation of Buildings

Overview

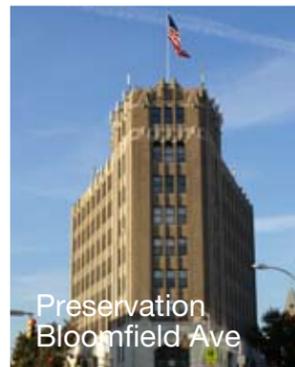
Preserving elements of Bloomfield's architectural heritage is a vital and valuable element of maintaining the unique and eclectic character of the downtown and is a key factor toward sustainability. Within the "BCRD-I" preservation and rehabilitation standards will be suggested for all existing properties.

Locating Opportunities for Preservation and Rehabilitation

The Building Preservation and Rehabilitation Map (Exhibit 14), identifies areas or groupings of buildings that may have opportunities for preservation or rehabilitation. Further study is required to determine the specific characteristics of each building.

The following are guidelines for determining preservation or rehabilitation potential:

- Over 50 years old
- Structural integrity
- Historic integrity
- Contributes to the overall historic character of the town
- Adaptability to new or improved use
- Symbolic significance related to significant event or person
- Visibility and prominence



Preservation & Rehabilitation of Buildings
5C.1



Mixed-use Rehabilitation



Rehabilitation with New Construction



Rehabilitation



Preservation



Rehabilitation with New Construction

Preservation & Rehabilitation of Buildings

(continued)

Guidelines

The National Park Service offers four approaches to the treatment of historic properties. Once the appropriate approach is determined, it should be followed consistently through the project. The first two listed, Preservation and Rehabilitation, are the most likely to be applied in the “BCRD-I”, though all four are appropriate. In each case, the goal is to bring out the character of the historic building. Preservation, rehabilitation, and restoration are also preferable from an environmental sustainability perspective in the re-use of existing structures and materials and reduced need for new materials and resource consumption.

- 1. Preservation** *“is defined as the act or process of applying measures necessary to sustain the existing form, integrity, and materials of an historic property. Work, including preliminary measures to protect and stabilize the property, generally focuses upon the ongoing maintenance and repair of historic materials and features rather than extensive replacement and new construction. New exterior additions are not within the scope of this treatment; however, the limited and sensitive upgrading of mechanical, electrical, and plumbing systems and other code-required work to make properties functional is appropriate within a preservation project.”*

There are several opportunities for this approach in the “BCRD-I”. This strategy involves the upkeep of an “intact” existing building with minimal interventions or changes to existing architectural.

- 2. Rehabilitation** *“is defined as the act or process of making possible a compatible use for a property through repair, alterations, and additions while preserving those portions or features which convey its historical, cultural, or architectural values.”*

There are several opportunities for this approach in the “BCRD-I”. It is strongly recommended to consider this approach to keep the existing character of the district intact while adding greater density. Projects involving new construction or new architectural elements, would be required to draw a clear and complimentary distinction between the existing and new elements. This contrast between existing and new can range from subtle to bold.

- 3. Restoration** *“is defined as the act or process of accurately depicting the form, features, and character of a property as it appeared at a particular period of time by means of the removal of features from other periods in its history and reconstruction of missing features from the restoration period. The limited and sensitive upgrading of mechanical, electrical, and plumbing systems and other code-required work to make properties functional is appropriate within a restoration project.”*

This method could be applied to historic buildings whose character has been marred by subsequent renovations and additions.

- 4. Reconstruction** *“is defined as the act or process of depicting, by means of new construction, the form, features, and detailing of a non-surviving site, landscape, building, structure, or object for the purpose of replicating its appearance at a specific period of time and in its historic location...”*

This approach could be applied to an area or building identified as benefiting from the reintroduction of an accurate historic picture.

- The redeveloper must first determine which approach is the most appropriate treatment for their building then consistently follow that approach throughout the course of the project.
- The National Park Service Guidelines must be followed within the “BCRD-I”. Redevelopers are required to consult those guidelines before any work begins.

<http://www.nps.gov/history/hps/tps/standguide/>

Existing Bloomfield



Alley Behind Washington Street



Alley Entrance on Washington Street

Potential Bloomfield



Outdoor Restaurant Seating



Decorative Paint & Finishes



Retail Storefronts



Access Ways

Sides and Backs of Existing Buildings

Overview

One of the unique characteristics of the Bloomfield Center Redevelopment District - Phase I is the existing alleys connecting various streets. These alleys and new pedestrian walkways will perform two critical functions:

1. Access to new parking structures; and
2. Mid-block access to Bloomfield Center destinations.

Alleys running along the sides and backs of buildings add interest to the downtown and offer pedestrians a sense of 'discovery' as they navigate. By treating the sides and rears of existing buildings with various architectural, artistic and rehabilitation techniques, this Redevelopment Plan strives to bring these alleys to life. For additional information refer to "Alleys and Linkages" page.

Guidelines

The following treatments are encouraged in the redevelopment area:

- Treating and or refurbishing existing exterior walls by repointing brick, power-washing, applying various architectural finishes or installing public art pieces/murals etc
- Refurbish doors and windows that front on pedestrian alleys, allowing for visibility into the retail spaces
- Providing architectural lighting on façades and architectural elements
- Provide various signage such as paddle or wall signs to activate alleys and parking areas adjacent to building



- 5D.1 Parking Location Access and Layout
- 5D.2 Ground Floor Parking Layout
- 5D.3 Loading Docks and Refuse Removal
- 5D.4 Services

*Design
Standards
& Form
Based
Zoning*

5

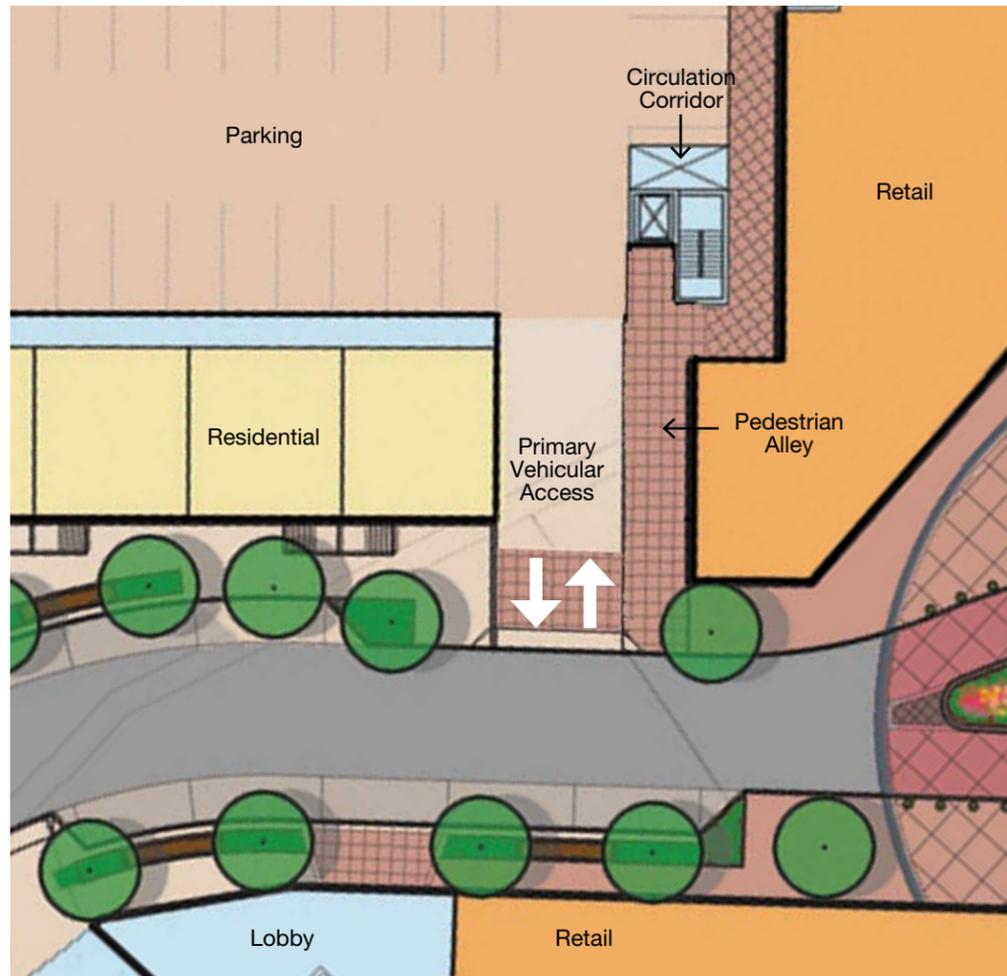


Diagram 1. Access Enlargement Example



Diagram 2. Access and Location (using Block 228 as example)

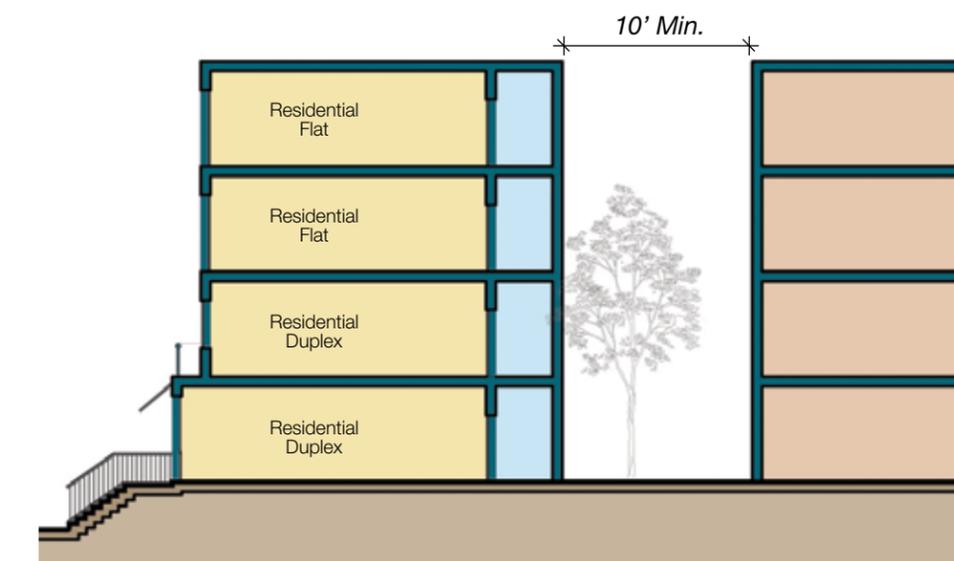


Diagram 3. Detached Garage Structure Example

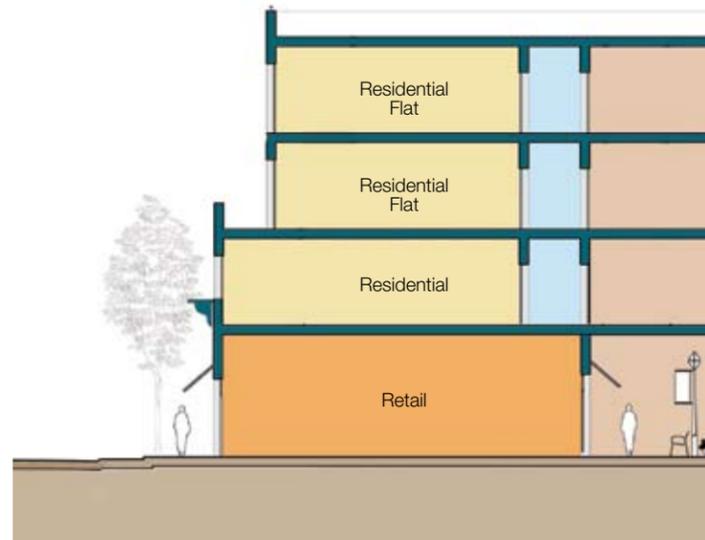


Diagram 4. Integral Garage Structure Example

Parking Location Access and Layout

Overview

Convenient shared parking facilities – for residents, visitors and commuters – are essential to the success of Bloomfield Center. However, the design of the parking facilities and how well they are integrated into the urban fabric will greatly effect the ultimate success of the downtown. This section will help guide in the planning and design of such facilities.

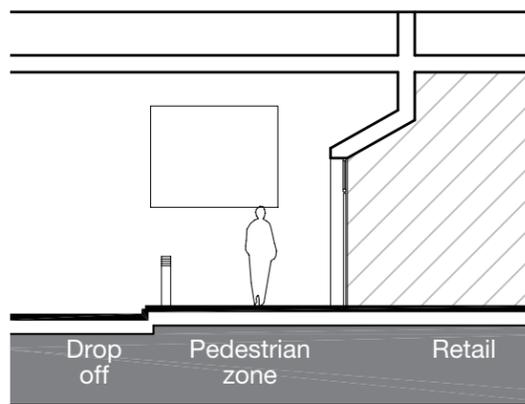
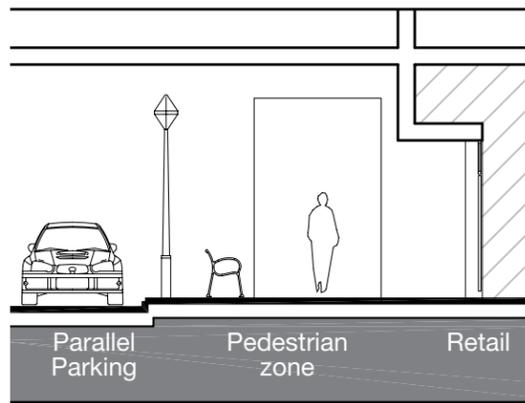
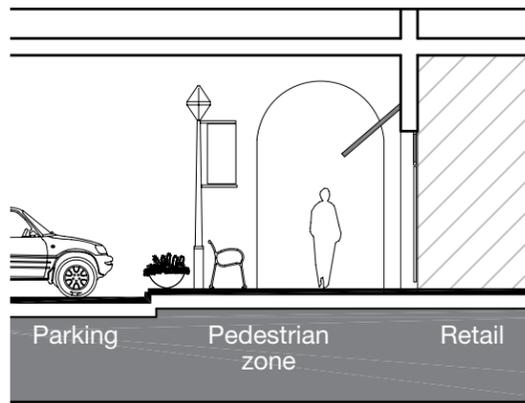
Requirements

- All parking in the district shall be located behind buildings and out of view from the street (Diagram 2).
- Small portions of garage structures may be exposed to the street if designed as an integral part of the overall architecture of the building and the opening does not exceed 40' in length as measured along the ROW line. No more than 2 openings permitted per side and they shall not be less than 100' apart.

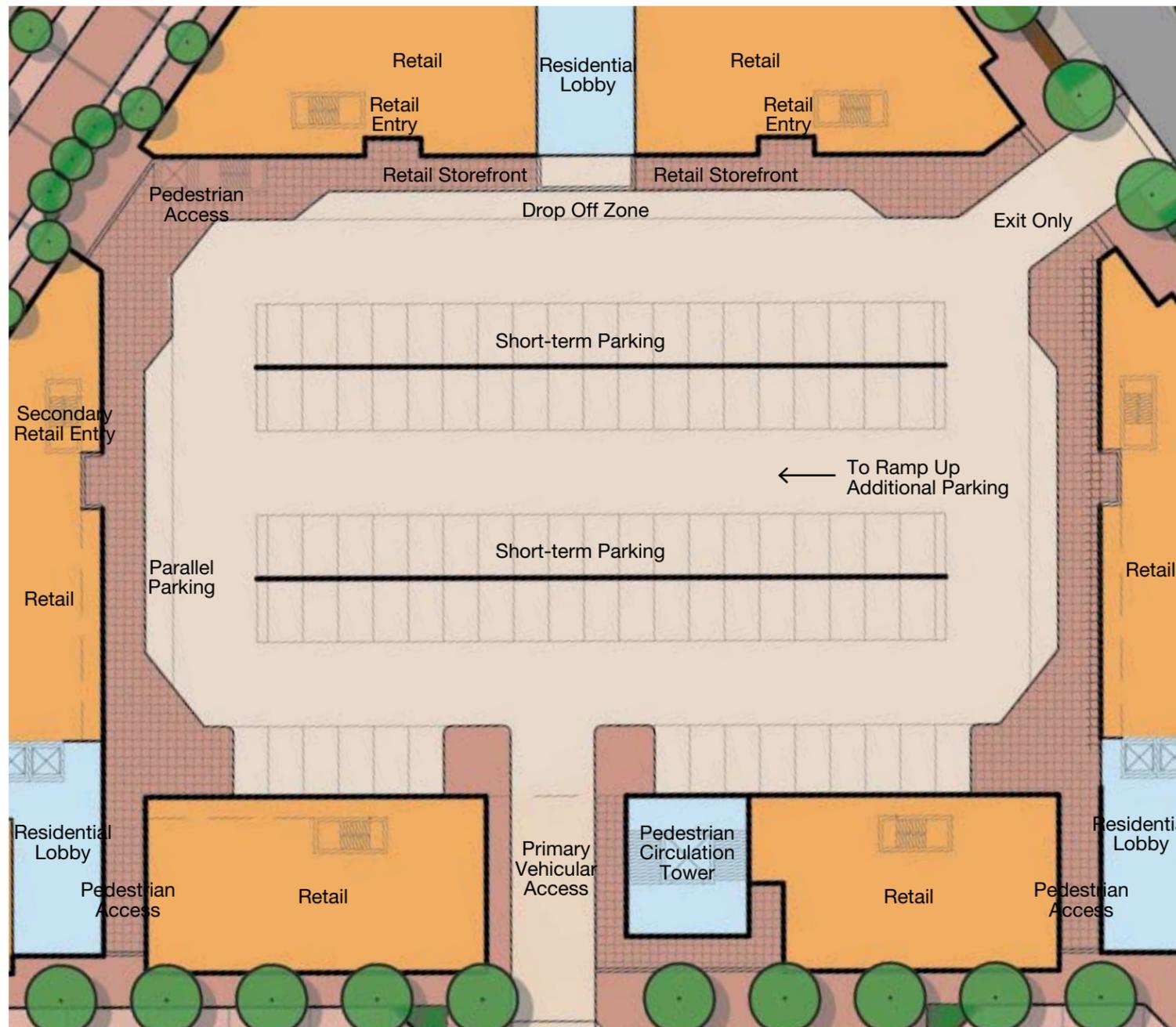
Guidelines

- Primary access to and from the garage should be provided from roads that are part of "The Loop" (Diagram 2).
- Multiple pedestrian access points should be provided in different areas of the garage to provide visitors with options to exit towards different destinations in the downtown (train station, shopping areas, residential entries etc).
- Parking Structures may be constructed as either integral to the main building which wraps the garage structure (Diagram 3) or detached from the garage structure (Diagram 4). If detached from the garage, a minimum of 10' separation should be provided.
- All access points, both vehicular and pedestrian, should be designed and detailed to properly announce this entrance. This includes: decorative pavement for sidewalks and driveway aprons from curb face to building face, pedestrian or wall mounted lighting, signage (Diagram 1)
- All vehicular access points should also provide a pedestrian sidewalk with a minimum width of 5', clear of obstructions. At no point should pedestrians be forced to use the drive aisle to exit the structure.
- Access widths and building openings should be kept to a minimum as to not negatively impact the street wall (22' Driveway – 5' sidewalk). In no case should the access opening exceed 40' in width unless caused by an unavoidable circumstance. No more than 2 permitted per side and they shall not be closer than 100' apart.
- Exterior top-level parking surfaces should strive to conform to cool roof guidelines, having a Solar Reflective Index equal to or greater than 78 for a minimum of 75% of the roof surface, or a vegetative roof for at least 50% of the roof area.
- Preferred parking spaces should be allocated and marked for use by sustainable transportation, i.e., carpool, compact, hybrid, electric, biofuel, and low-emissions vehicles.





Ground Floor Parking Interior Layout Options



Ground Floor Parking Layout (using Block 228 as example)

Ground Floor Parking Layout

Overview

Attracting and expanding regional, local and national retailers is an important element to the success of this plan and ultimately, Bloomfield Center. The parking structures and the way that they interact with the street, retail and pedestrian experience will play a critical role. In contrast to most typical parking structures, the ground floor of the parking structures in this plan should act as an extension of the streetscape. Comfortable, decorative sidewalks; attention to architectural detail; and double-sided retail with an interior storefront entry are a few of the interventions that can help achieve this goal. This concept will extend the shopping season, provide easy convenient access to retail, provide pedestrians with protection during inclement weather and create a comfortable, seamless pedestrian environment.

Guidelines

The following guidelines are encouraged for the ground floor parking and associated retail spaces:

- All retail adjacent to ground floor parking (surface or structured) should provide "Dual Storefronts". One storefront entry on the street and one storefront entry in the parking structure, unless this is deemed to be cost prohibitive.
- Continuous, ample sidewalks (8' Min.) should be provided along all interior retail Façades. Sidewalks should be a decorative material and should be easily differentiated from the vehicular lanes.
- Walls and ceilings adjacent to and above the pedestrian sidewalks should be treated with architectural finishes equivalent to those used on an arcade or colonnade.
- Lighting within the ground floor should be limited to "white light" sources. High Pressure Sodium lighting should not be used on the ground floor.
- Large, high wattage "flood lighting" or "site lighting" typically found in parking structures should not be used to light sidewalk and storefront areas. Lighting sources in these areas should be pedestrian scale lighting.
- Pedestrian elements typically found in the streetscape, including but not limited to, pedestrian scale lighting fixtures, wayfinding signs, benches, bicycle racks, planters (if possible), etc should be provided along the sidewalks.
- Retail spaces that front on both the street and parking structure/surface parking lots providing "Dual Storefronts" are permitted to apply the requirements set forth in the Bloomfield Zoning Ordinance 315-42 Signs 'A. General Provisions' and 'B. Signs Permitted in Business Zones.'
- The sidewalks and pedestrian circulation should have clear and visible access and connection to the street. Dead-end sidewalks should be avoided. If a sidewalk must end without continuing around the perimeter of the parking, an exit or connection to the street should be provided.

Illustration of Retail Façades Inside Parking Structures



Pedestrian Amenities



Two Sided Retail

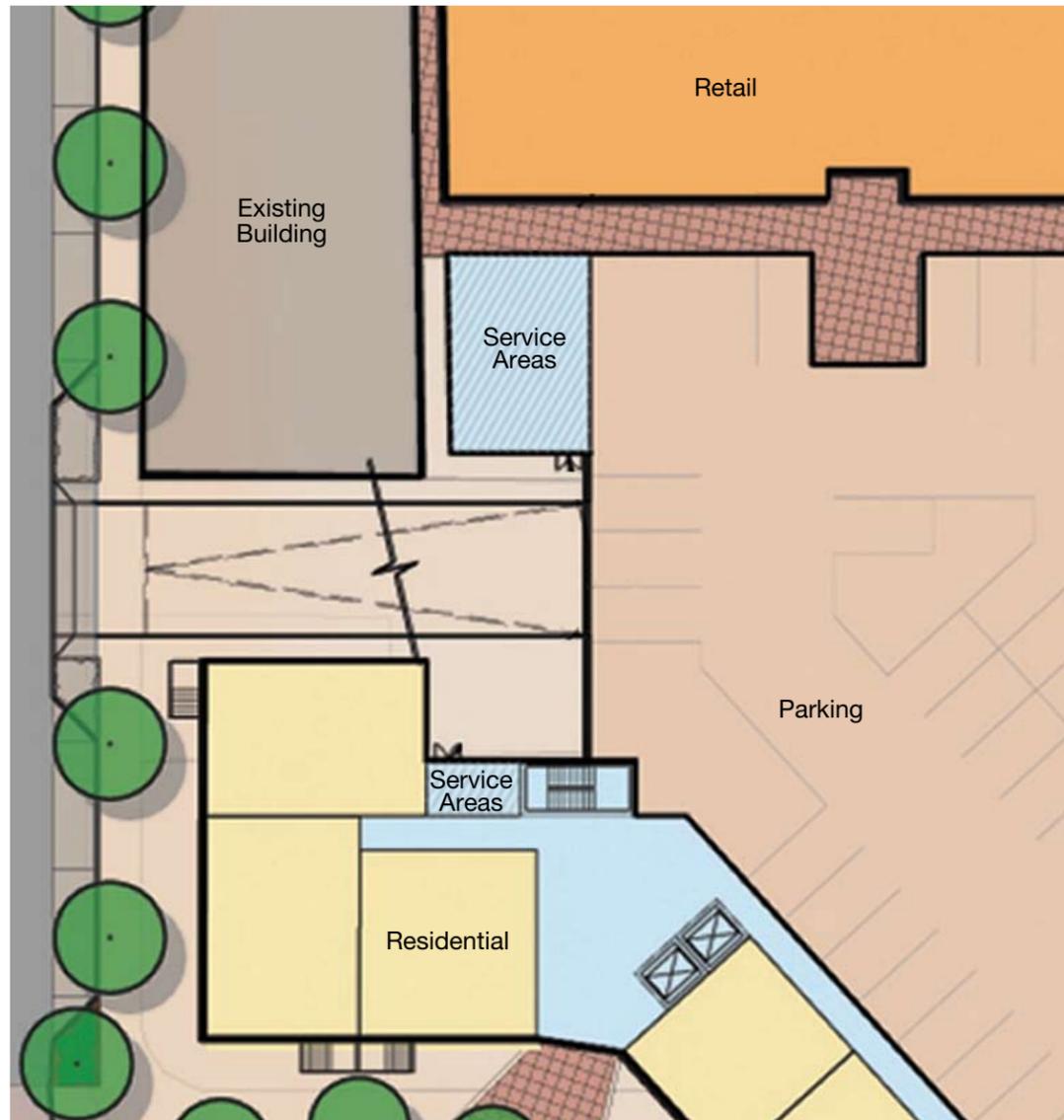


Designed & Detailed Entry

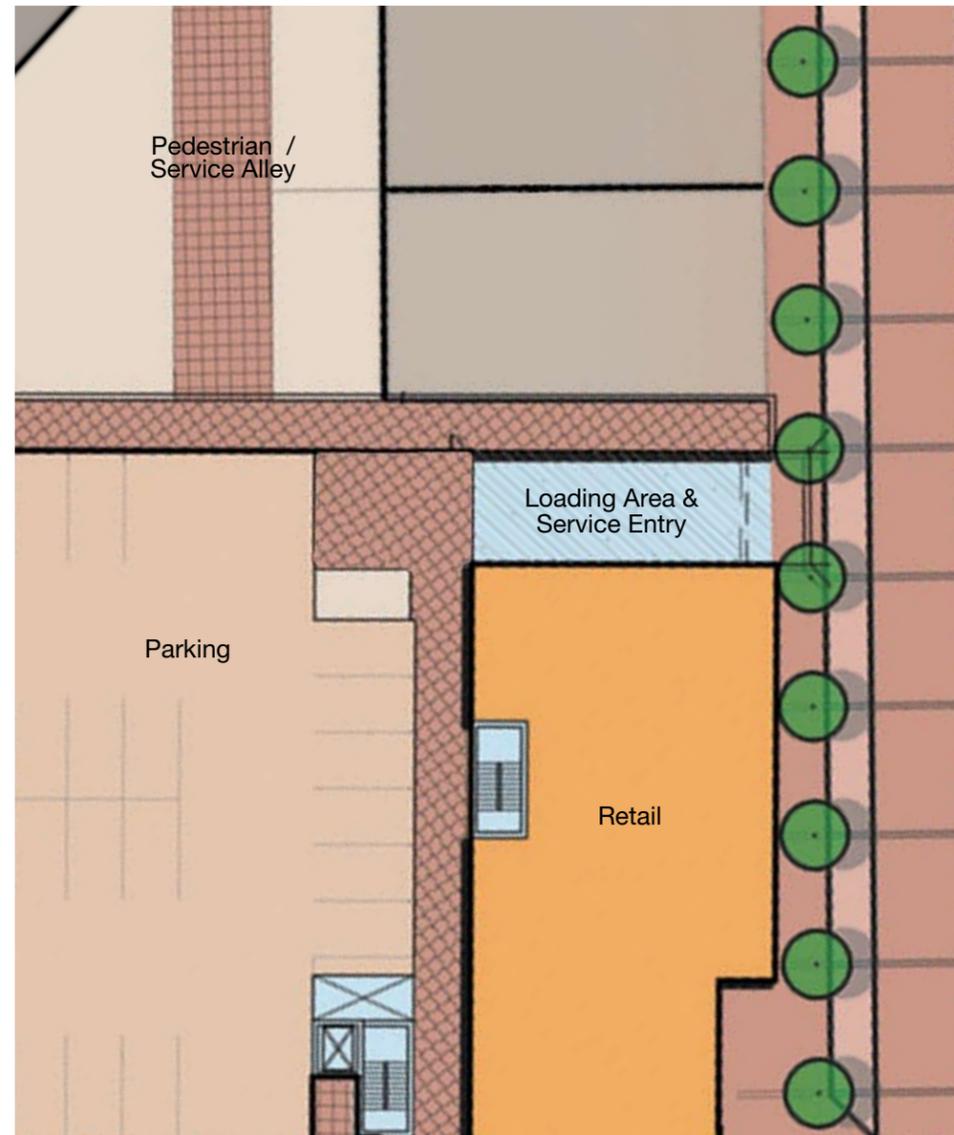


Interior "Streetscape"





Service / Utility Spaces



Loading Area

Loading Docks and Refuse Removal

Overview

Loading docks, garbage and recycling storage and disposal are essential to the functionality of new development. It is important that the functionality of these areas are balanced with the aesthetic and other impacts that these areas may have. Buildings should be organized so that these elements are designed into the buildings or landscape so that their impact on streets, open space and adjacent homes are minimal.

Guidelines

- Layout of refuse and recycling areas should be done in a way that meets township servicing requirements unless other accommodations for removal are being proposed.
- Locate loading, refuse and recycling and other services so they do not negatively impact adjacent residents or property owners. Screen using fencing and/or landscaping or integrate into the design of the building so that they are not visible from the street, open spaces or adjacent residences.
- Whenever possible, loading, refuse and recycling removal areas should be consolidated to one area and shared by multiple or all tenants/landowners.



Building Entrance with Garage



Loading Dock



Vehicular Access



Trash Enclosure Gate



Service Entry





Mechanical Equipment Not Set Back from Roof Line



Mechanical Equipment 'Littering' the Rooftop



Mechanical Equipment Screened from View by a Parapet Wall



Mechanical Equipment Set Back from the Roof Line



Mechanical Equipment Placed at the Rear of the Roof



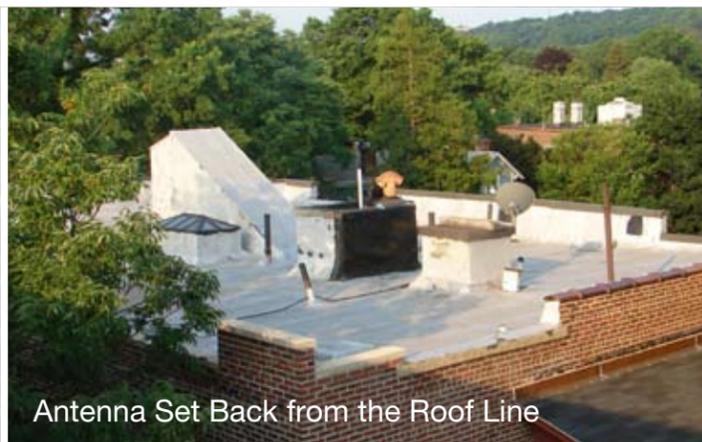
Antenna Not Set Back from Roof Line



Antenna Visible from Street



Antenna Placed at the Rear of the Roof



Antenna Set Back from the Roof Line

Services

Overview

Utilities, transformers, meters, HVAC, alternative energy production and other equipment are elements that are essential to constructing modern, efficient new development. Additional public utilities such as wireless communication facilities (cellular towers) have become a part of the urban fabric that also need to be addressed when designing and developing a downtown. These elements should be located carefully as to avoid negatively impact the views and vistas from streets, open space and residential areas throughout the downtown.

Rooftop Utilities

Guidelines

- All rooftop utilities shall be located as to minimize visibility from adjacent streets, sidewalks and open space.
- Methods for minimizing visibility may include setting utilities back from roof-line, locating at the rear of the roof-line and screening.
- Screening may be provided but should be designed as an integral part of the building's architecture. (i.e. It should not be a stand alone, fenced in area on the roof)

Wireless Communication Facility

Antennas located on towers or monopoles shall not be permitted in the "BCRD-I". All applicants seeking to install cellular communication antennas within the "BCRD-I" are subject to the standard project approvals process as outlined in this plan. All Telecommunications facilities located on top of buildings and structures within the redevelopment area should meet the following guidelines:

Guidelines

- Avoid any potential adverse impacts to residential areas and land uses
- Minimize the number of towers, monopoles and antennas throughout the community
- Investigate the potential of utilizing existing tower/monopole sites prior as a primary option rather than constructing new, single facilities. The applicant should demonstrate that no other site, existing tower or structure or alternative technology that does not require the use of towers or structures and is not within the redevelopment area is available that can accommodate the applicant's targeted market coverage area in full compliance with the township zoning ordinance.
- Utilize innovative design, planning, siting, screening and/or camouflaging techniques
- Consider public health and safety as it relates to wireless communication requirements.

- Antennas will be limited to buildings above 4 stories.
- When mounted on a building, the antenna and supporting electrical and mechanical equipment should be of a neutral color that is identical to, or closely compatible with, the color and design of the building and should be designed to be as unobtrusive as possible.

Submission Requirements (Wireless Communication Facility)

In addition to the information required by the Mayor and Council as outlined in the project approvals section of this plan, applicants must provide the following:

- Visual Impact Analysis
 - Computer Generated Architectural renderings or "before and after" photo montages of the antenna and all ancillary components.
 - A statement, certified by the applicant, to assure that the proposed antenna and its ancillary elements will be placed in a reasonably available location which will minimize the visual impact of the surrounding area
- Radio Frequency Coverage Analysis
- Inventory of Proposed and Existing Sites within 1/2 mile of township (including location, height and design of each facility)
- Availability of alternative structures – A written report that sufficiently demonstrates the absence of any other available options.
- Availability of Alternative Technology – Demonstrate that technologies that do not require building mounted antennas are not suitable for this application. Excessive cost shall not be considered a "sole" reason for determining a technology unsuitable.



A Comfortable Environment for Pedestrians

- 5E.1 Street Typology
- 5E.2 Streets
- 5E.3 Alleys and Linkages

*Design
Standards
& Form
Based
Zoning*

5

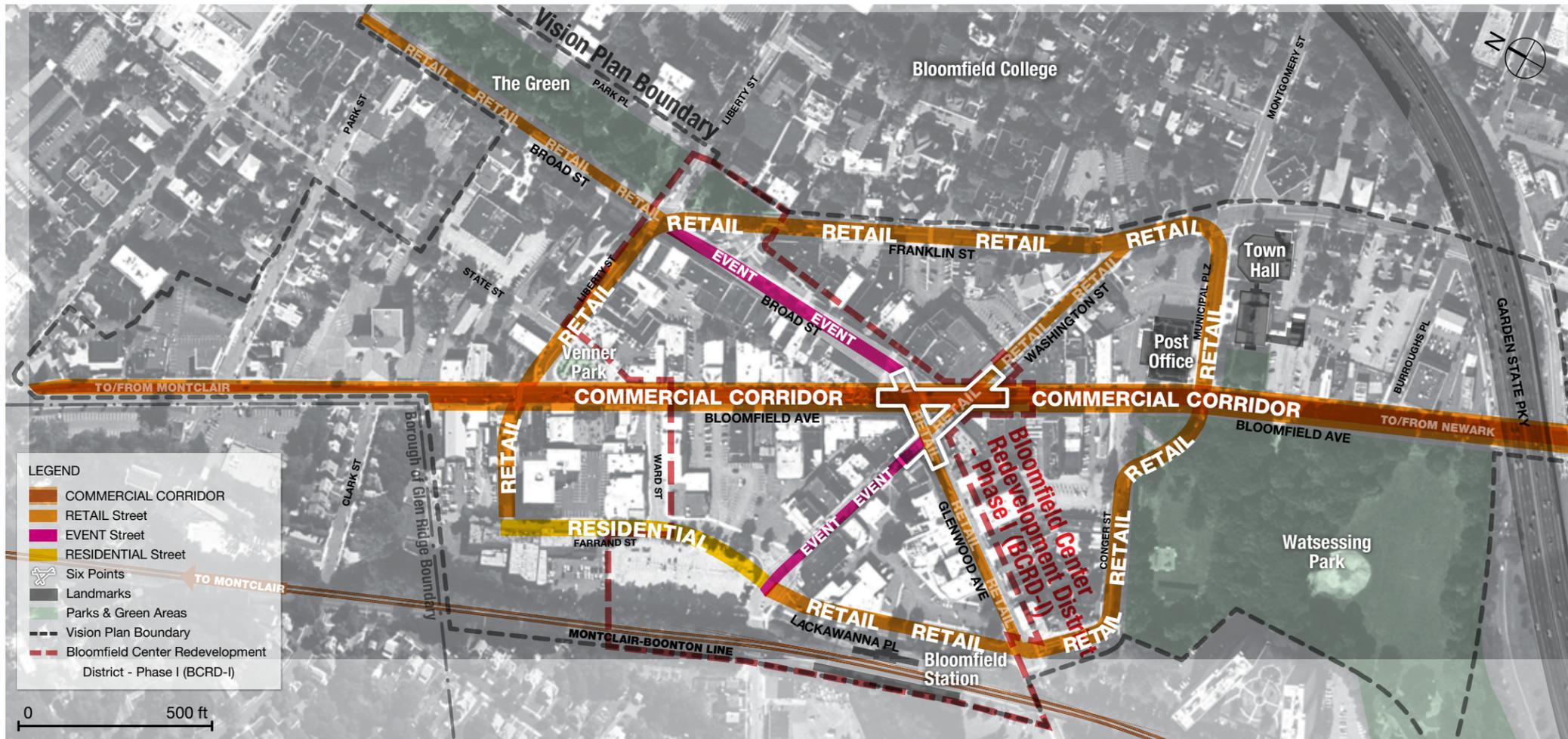


Exhibit 15. Street Typologies

	Use	Sidewalk Width	Cartway	Bike
Commercial Corridor	<ul style="list-style-type: none"> Active Pedestrian Cars 	10' - 15' (Ideal)	<ul style="list-style-type: none"> 4 Lanes 44' Width On Street Parking 	"Share the Road" Signs
Event Street	<ul style="list-style-type: none"> Active Pedestrian Cafes Events Bicycles 	Flexible	<ul style="list-style-type: none"> One Way / Two Way 16'-30' Width Flexible Parking Lane 	None
Residential Street	<ul style="list-style-type: none"> Passive Pedestrian Bicycles 	6' Planting 6' Paved	<ul style="list-style-type: none"> 2 Lanes 24' Width On Street Parking 	* "Share the Road" Signs
Mixed Retail	<ul style="list-style-type: none"> Active Pedestrian Passive Pedestrian Bicycles 	6' - 10'	<ul style="list-style-type: none"> 2 Lanes 24' - 30' Width On Street Parking 5' Bicycle Lanes on Broad Street 	* "Share the Road" Signs Bicycle Lanes on Broad Street

Street Typology

Bloomfield Center is characterized by a unique assemblage of streets that come together at the historic "6-Points" intersection. Each of these streets has a distinct character and 'vibe' that is derived from the physical characteristics - street width, location, buildings lining the streets, scale and usage. For simplicity, these streets have been grouped into four "Street Typologies":

- Commercial Corridor** – Envisioned as an exciting, urban boulevard that maintains a high level of service for vehicles traveling through the area, but balances that with an inviting pedestrian environment that provides a comfortable shopping experience.
- Event Street** – The funky, fun and unique areas of the downtown, these streets are geared towards pedestrians and maintain very low vehicular volumes. These streets may have the ability to be closed to cars completely in order to host seasonal events. The "Flexible Street" concept, outlined below will keep this street and its use interesting and dynamic.
- Residential Street** – A transformation of "industrial" back roads into lush tree-lined green residential streets lined with stoops and front yard gardens.
- Mixed Retail** – The common downtown street with comfortable sidewalk widths, ample street trees and seating areas, perfect for informal meetings with neighbors while running your daily errands.

The following actions and improvements are recommended as part of this plan:

General Improvements

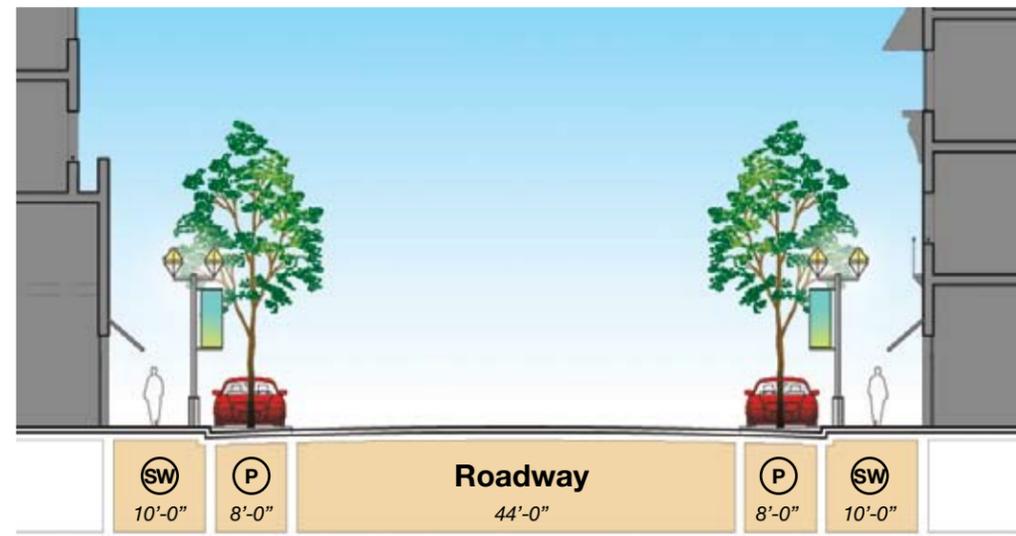
- Lighting** – Consistent street and sidewalk lighting comprised of pedestrian scale fixtures approximately 16' in height. Throughout the district, these fixtures will be single head, post top style fixtures. On Bloomfield Ave, the same fixture will be used in a post top double "T" configuration appropriate for creating a "boulevard" feel, distinguishing this portion of Bloomfield Avenue and creating a "sense of arrival" in Bloomfield Center. Lighting will be designed to be energy efficient and prevent light pollution
- Sidewalks** – Ample continuous sidewalks throughout using well designed, decorative pavement. ADA compliant curb ramps at all intersections and crossings
- Street Trees** – Street trees at no greater than 40' O.C. except on Bloomfield Avenue. All trees should be limbed up to 8' Min.
- Furniture** – Benches and Trash/Recycling receptacles should be located in coordination with street trees and lighting to create comfortable seating and gathering spaces for pedestrians.
- Street Furniture** – Existing street furniture should be reviewed and coordinated with BCA. If new or in good condition, matching or building upon this furniture should be considered.
- Wayfinding System** – A clear and comprehensive way finding system should be developed. This should be done in coordination with efforts currently underway by BCA and outlined in the "Bloomfield Center Streetscape and Circulation Guidelines", dated April 30, 2004.

* Bike Lane to be considered for "The Loop"

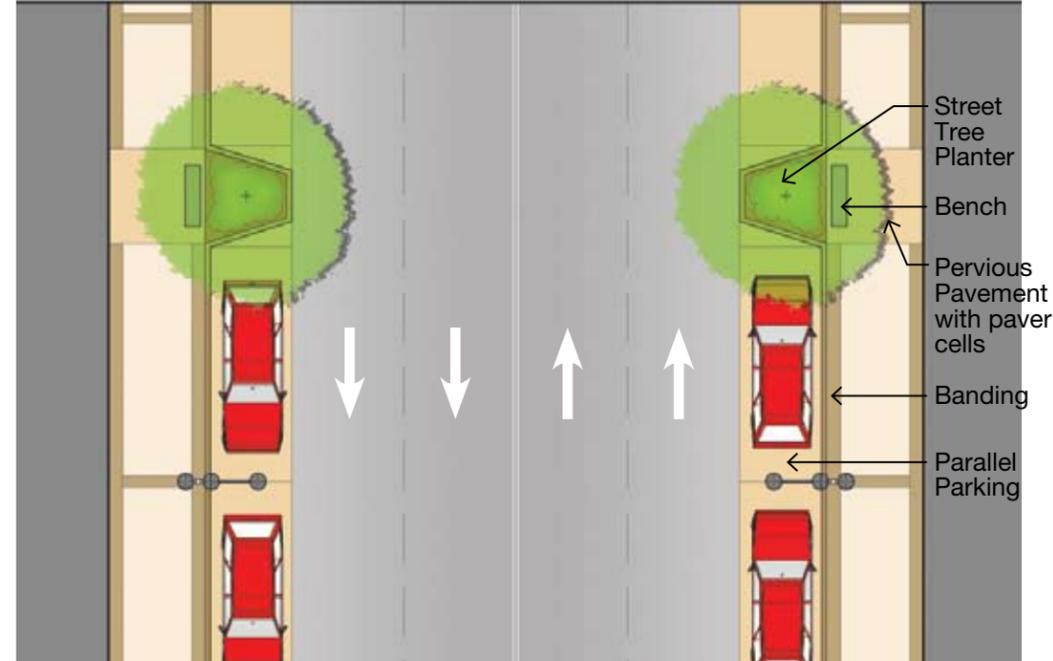
Street Typology

5E.1

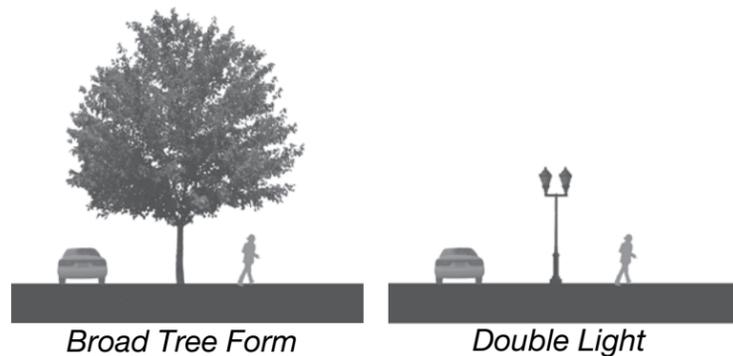
Commercial Corridor



Section ← 80'-0" →



Plan

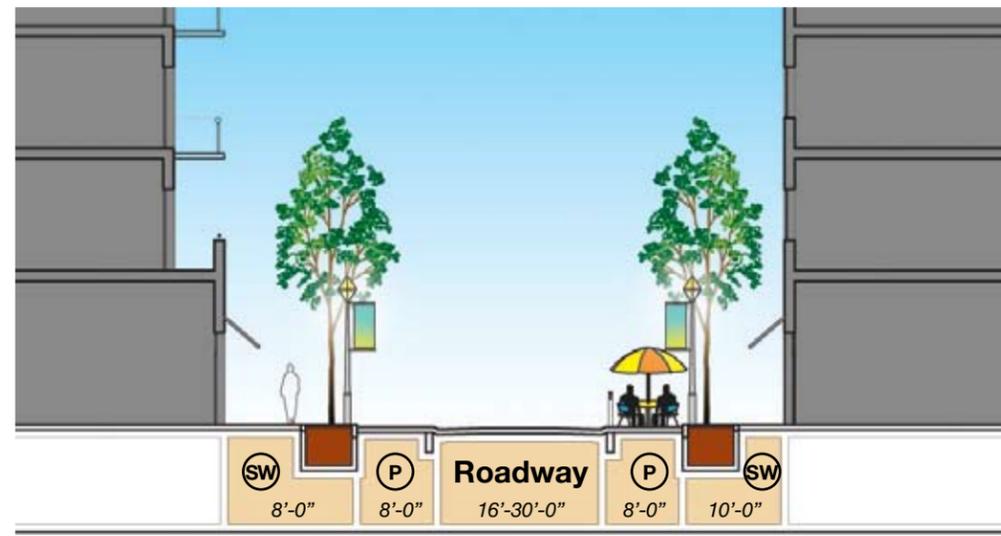


Details

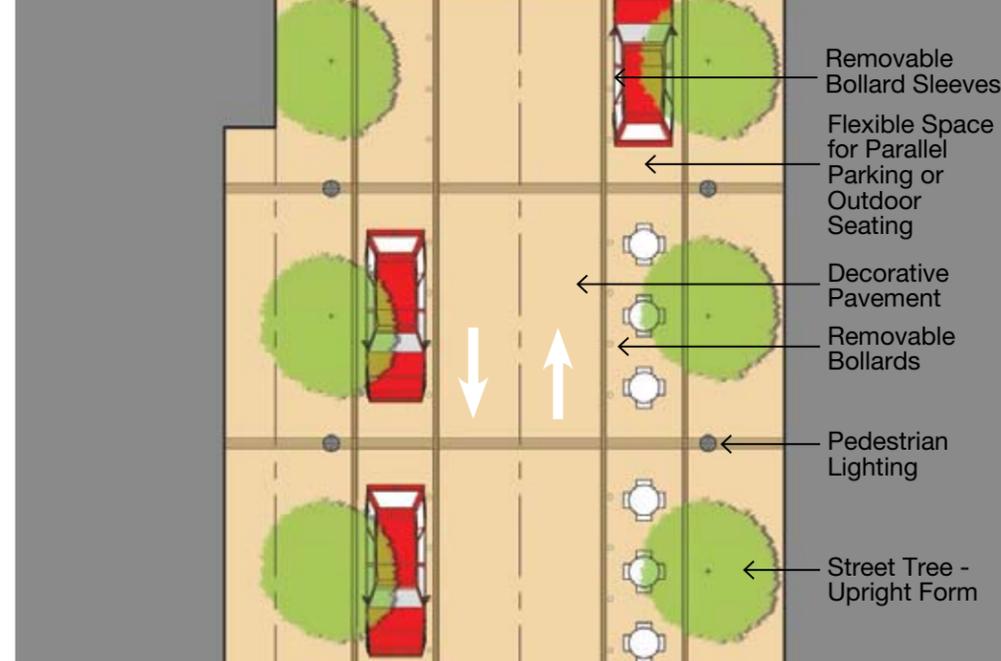
Key Elements

- Bump out for Trees & Amenities
- Pedestrian Lights
- Clear Sidewalks

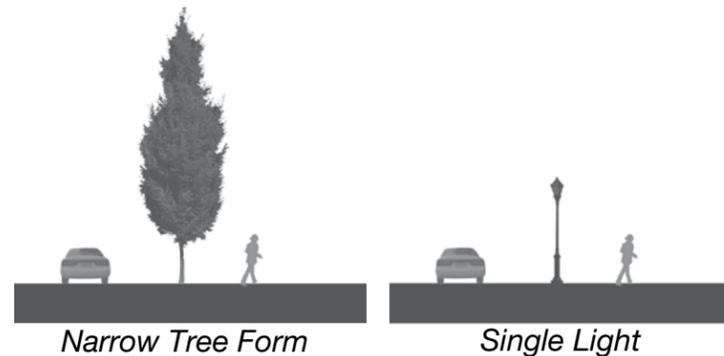
Event Street



Section ← 50'-65'-0" →



Plan



Details

Key Elements

- Outdoor Cafes
- Ability to Host Functions
- Flexible Parking Lane

Streets

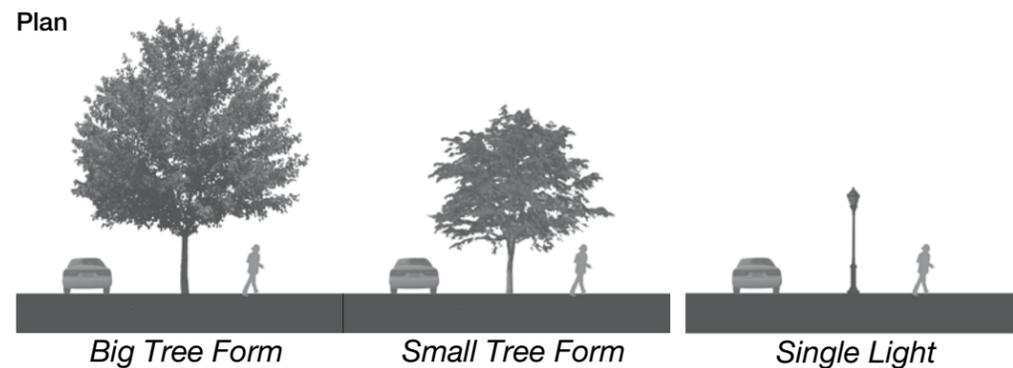
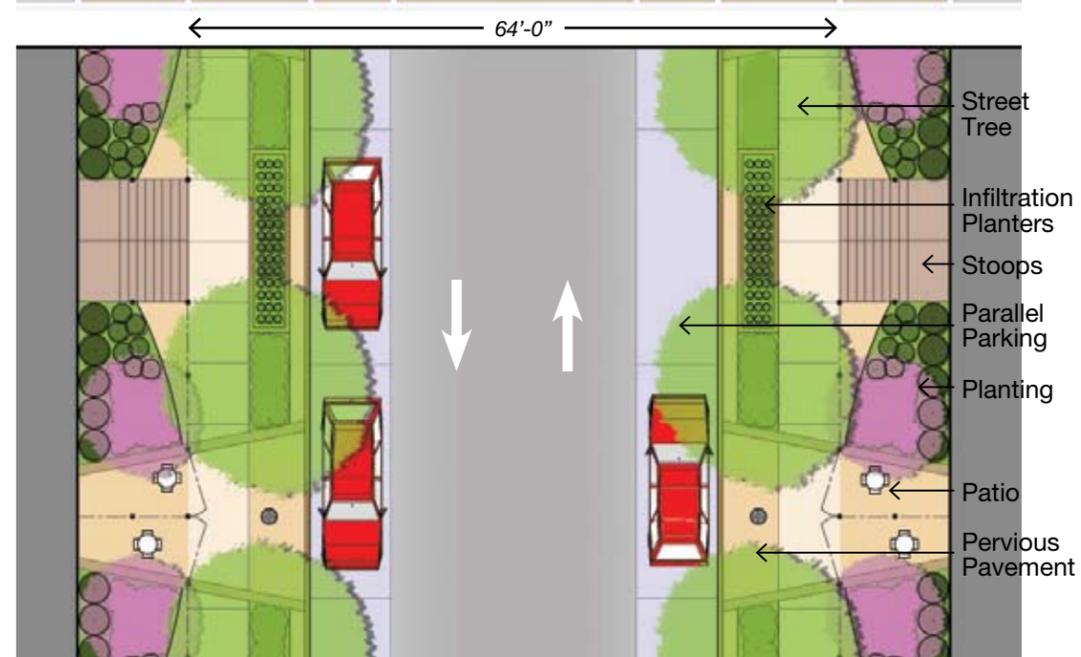
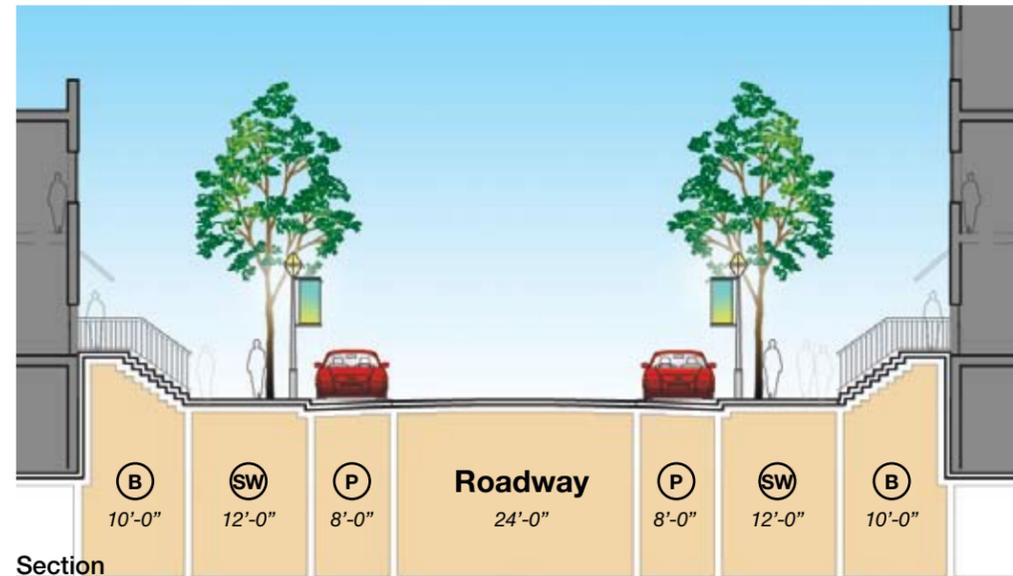
Commercial Corridor

- **Lighting:** In lieu of traditional “Acorn Drop” style street lighting, a pedestrian scale (+/- 16’ Max. height), double headed “boulevard” style fixture should be used in order to distinguish this portion of Bloomfield Avenue from adjacent towns. Lighting will be designed to be energy efficient and prevent light pollution.
- **Sidewalks:** Concrete with brick or concrete paver banding with distinct paving patterns at “pocket parks”, seating areas, alley entries etc. Wherever possible, sidewalk should be expanded to an ideal 15’ width. Building owners should be encouraged to “setback” their building from the property line (as permitted) and the additional space paved as expanded sidewalk area.
- **Street Trees:** Due to minimal sidewalk widths, curb bump outs will be constructed to accommodate street trees and seasonal plantings. These “bump outs” will be provided every two parking spaces as possible. Tree form should be broad, but light in texture to provide maximum canopy while ensuring that they do not hinder visibility of adjacent retailers.
- **Furniture:** Benches and Trash/Recycling receptacles should be located in coordination with street trees to create comfortable seating and gathering spaces for pedestrians.
- **Bicycle Facilities:** “Share the Road” signs should be designed and located frequently along the roadway with bike racks located within 100’ of each building entrance.

Event Street

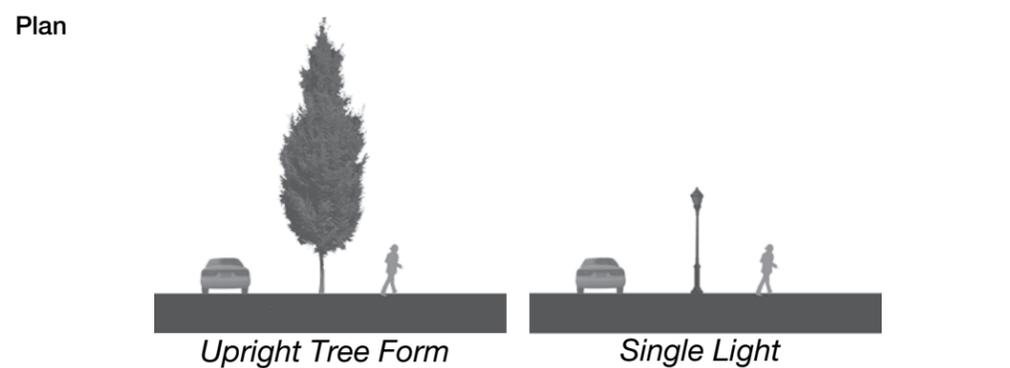
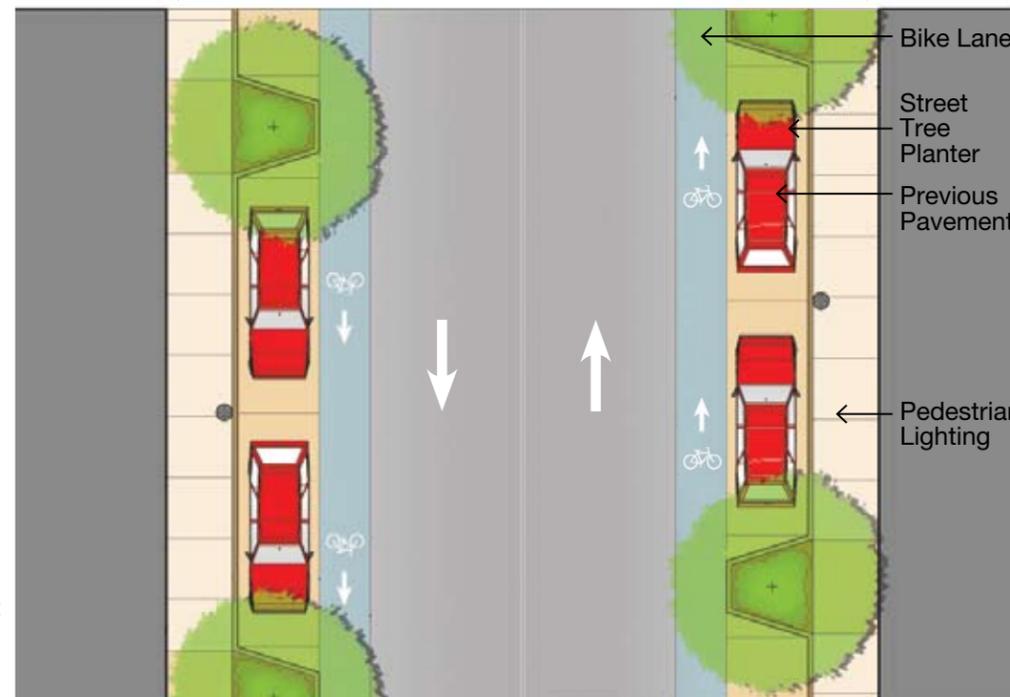
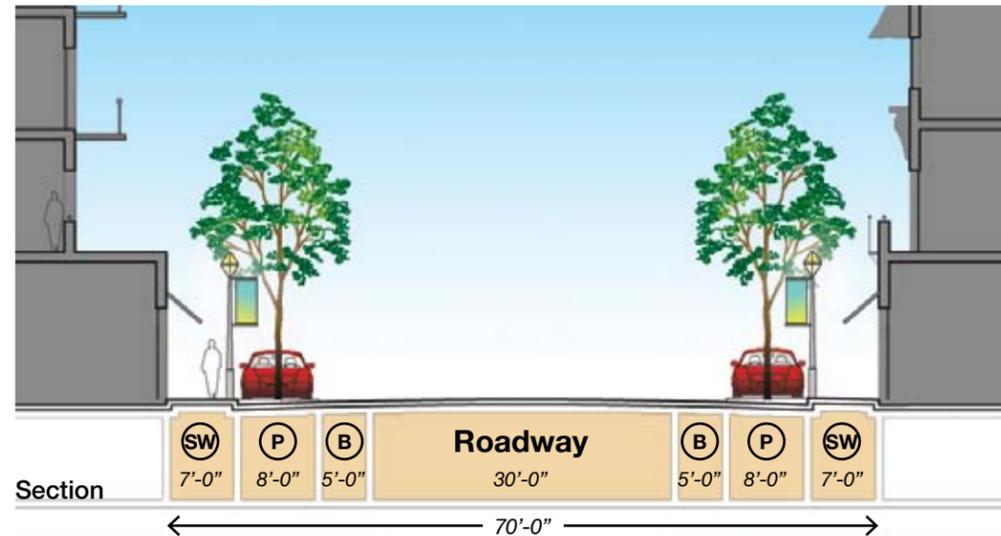
- **Lighting:** Single headed pedestrian scale fixtures. Accommodations should be provided for mounting banners. Electrical outlets should be provided at both the pole top and base to accommodate vendors and seasonal lighting. Lighting will be designed to be energy efficient and prevent light pollution.
- **Sidewalks:** The sidewalk should be paved using a decorative brick paver or equal.
- **Street Trees:** Trees should be located at 25’-30’ on center. Tree form should be upright and light in texture to provide ensuring that they do not hinder visibility of adjacent retailers.
- **Furniture:** Benches and Trash/Recycling receptacles should be located frequently throughout the street in groupings to create small gathering spaces.
- **Flexible Street Concept:** The “parking lanes” on either side of Washington Street should be designed as “flexible space” that can either be used for parking or for programming (i.e. outdoor café seating, vendor carts). Removable bollards will be installed along the “inside” of the lane. These bollards may be installed or removed by the town or BCA in order to delineate certain areas for use other than parking. This maximizes the use of all areas of the street, adds excitement to the street, accommodates a variety of seasonal and non seasonal uses and ultimately creates an interesting, dynamic streetscape and shopping experience.

Residential Streets



- Details**
- Key Elements**
- Green Street Edge
 - Stoops & Frontyard Gardens

Mixed-Retail Streets



Streets

(continued)

Residential Streets

- **Lighting:** Single headed pedestrian scale fixtures. Lighting will be designed to be energy efficient and prevent light pollution.
- **Sidewalks:** Sidewalks in the residential streets should be comfortable, lush streets with large street trees and planting areas. As "Green streets" these streets will accommodate "infiltration planters" – a sustainable stormwater collection and infiltration method – along the curblines.
- **Stoops:** All residential units facing onto the residential streets should have front stoops that engage the street.
- **Street Trees:** Trees should be located at 25'-30' on center. Tree form should be broad and provide ample shading of the sidewalks and streets.
- **Furniture:** Benches should be located intermittently along the street to provide resting places for residents. Trash/Recycling receptacles should be located in coordination with street trees and benches to encourage proper street waste disposal.
- **Bicycle Facilities:** "Share the Road" signs should be designed and located frequently along the roadway. Bike racks should be located within 100' of each building entrance.

Mixed-Retail Streets

- **Lighting:** Single headed pedestrian scale fixtures. Accommodations should be provided for mounting banners. Electrical outlets should be provided at both the pole top and base to accommodate vendors and seasonal lighting. Lighting will be designed to be energy efficient and prevent light pollution.
- **Sidewalks:** Concrete with brick or concrete paver banding with distinct paving patterns at "pocket parks", seating areas, alley entries etc. Wherever possible, sidewalks should be expanded to an ideal 10' width. Building owners should be encouraged to "setback" their building from the property line (as permitted) and the additional space paved as expanded sidewalk area.
- **Street Trees:** Where sufficient width exists, street trees should be accommodated within the sidewalk area. In areas of minimal sidewalk widths, curb bump outs will be constructed to accommodate street trees and seasonal plantings. These "bump outs" will be provided every two parking spaces as possible. Tree form should be upright and light in texture to provide maximum canopy while ensuring that they do not hinder visibility of adjacent retailers.
- **Furniture:** Benches should be located intermittently along the street to provide resting places for residents. Trash/Recycling receptacles should be located in coordination with street trees and benches to encourage proper street waste disposal.

- **Bicycle Facilities:** Continuous 4'-5' wide bicycle lanes should be provided on both sides of the street whenever sufficient width can be made available. This is particularly important on Broad Street, linking the 6-Points intersection to the many destinations along Broad Street (particularly The Green). Bike racks should be located within 100' of each building entrance.



Alleys and Linkages

Overview

The street grid in downtown Bloomfield has a distinct character similar to that of European streets and alleys. Building on this existing infrastructure, pedestrian alleys should be promoted as an integral component of the overall hierarchical system of movement and connection throughout the downtown. The purpose of these alleys is to connect pedestrians to parking, streets and other significant destinations. Alleys give pedestrians the option for continuous movement with minimal interruption by vehicular traffic.

Guidelines

Alleys within the Redevelopment Area should:

- Design new construction and renovations to accommodate key pedestrian alleys and linkages (Exhibit 16. Alleys and Linkages)
- Connect parking to retail streets and destinations
- Provide signed entries, with wayfinding signage
- Provide ample light with a variety of lighting types such as paver lighting, wall sconces and catenary lighting to ensure a pleasant and safe experience for the user
- Serve as secondary access points to various destinations
- Provide well lit, interesting store windows, paddle signs, etc to achieve the alley.
- Provide varying entry designs such as arbors, gates, archways, etc
- Always connect through to public streets and never end in a “dead end”

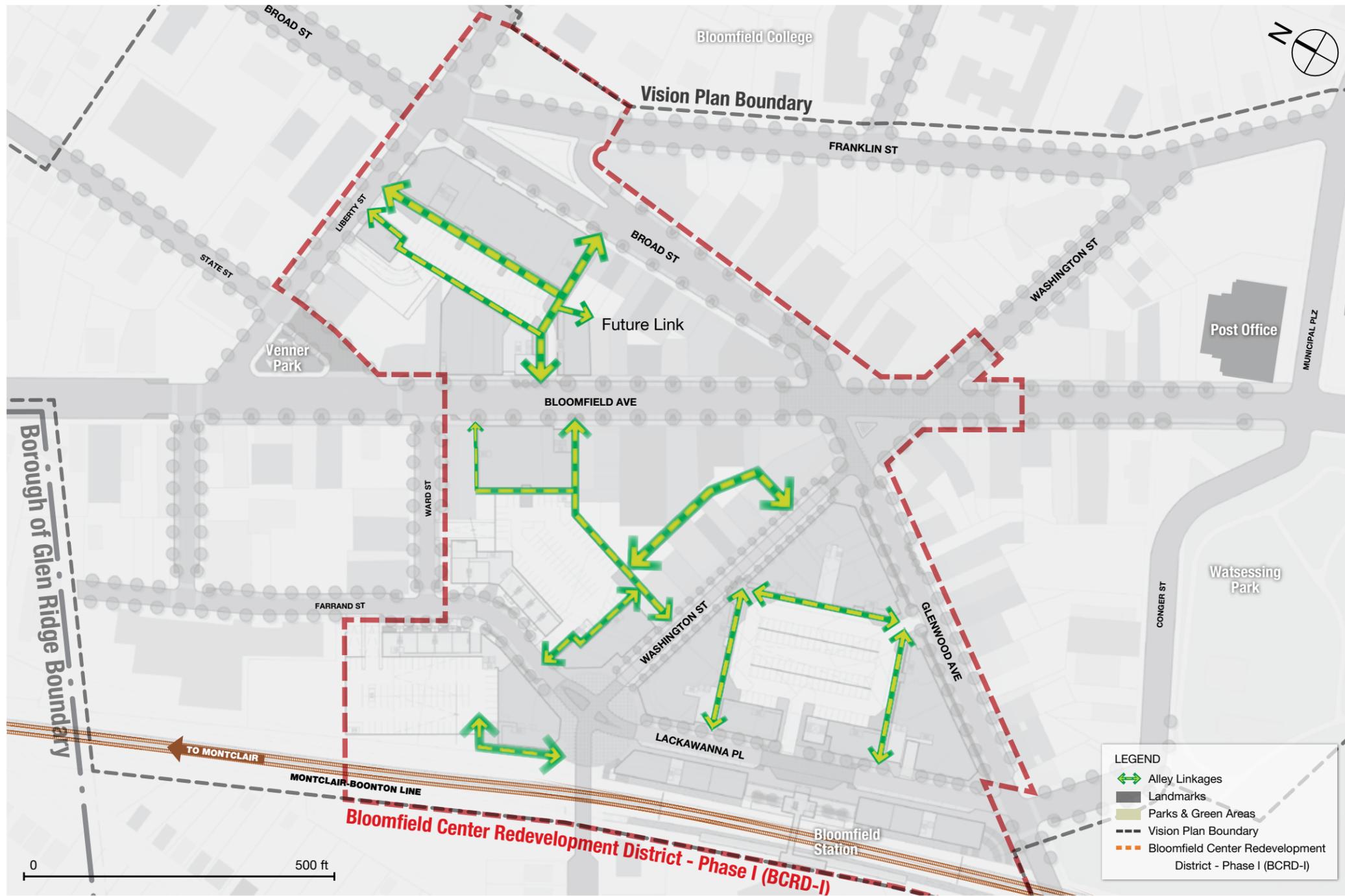


Exhibit 16. Alleys and Linkages

Refer to Schedule A for precise location of “BCRD-I” boundary



Alleys and Linkages

5E.3

Greener Bloomfield Sustainability Guidelines

This section lays out the specific strategies to meet the community's objectives to have a green and sustainable downtown. Green and sustainable design however is not limited to this section. Many of the principles found throughout this plan contribute to a sustainable development.

6.1 Sustainable Guidelines



Sustainable Guidelines

Overview

“Sustainability : Meeting the needs of the present without compromising the ability of future generations to meet their own needs”

The Bloomfield Center Redevelopment Plan - Phase I embodies a strategy for achieving “A Green Downtown”. This strategy was developed in coordination with the “Greener Bloomfield” task force. The plan addresses the town’s long term goals of economic, social and environmental sustainability, often referred to as the “Triple Bottom Line”.

“The Triple Bottom Line”:

1. **Economic Sustainability:** Produce an environment that is financially attractive for new and existing businesses, residents, visitors, and the township with good jobs, compelling destinations, lower long-term operating costs, and an improved tax base.
2. **Social Sustainability:** Provide a liveable, attractive and inclusive community
3. **Environmental Sustainability:** Reduce the overall demands on local infrastructure systems, the use of natural resources and sources of pollution

Strategies:

- Establish and encourage appropriate minimum/maximum densities to conserve land and reduce automobile dependency
- Provide space for high quality commercial tenants
- Design convenient shopping and entertainment to attract business and foster social interaction
- Capitalize upon the train station as a centrally located public service and amenity
- Reduce automobile dependence and congestion, improving air quality and quality of life
- Improve and expand pervious landscaped zones to mitigate heat island effect and improve stormwater management capabilities
- Provide comfortable pedestrian areas including public amenities, landscaped open spaces and high quality streetscapes fostering community activity and a healthier environment
- Reduce township and building operating costs through higher performance buildings
- Reduce water consumption and conveyance, including sewage, and improve storm water management systems
- Reduce the demand for fossil fuels and encourage the use of alternative energy sources
- Reduce the use of polluting materials, lighting and activities thus improving air quality and conserving the quality and quantity of natural resources

Sustainable Guidelines
6.1

Approach

Bloomfield aims to address the specific needs and be a leader in sustainable redevelopment by:

- Adopting New Jersey’s Smart Growth strategies as the basis of the Redevelopment Plan
- Shaping the Redevelopment Plan to take full advantage of the existing sustainable features of the redevelopment area including:
 - The proximity to Bloomfield’s train station and the Garden State Parkway
 - The existing building stock and residential density
 - Watsessing Park and The Green
- Aligning the Redevelopment Plan and the requirements for potential developments within the Redevelopment Plan with the U.S. Green Building Council’s LEED® (Leadership in Energy and Environmental Design) Green Building Rating Systems. The USGBC, a recognized leader in environmentally responsible buildings and environments, established LEED® as a voluntary national rating system
- Incentivizing new and existing buildings to obtain third party certification for sustainable performance
- Identifying Bloomfield’s specific infrastructure and environmental issues and addressing them within the design of the Redevelopment Plan as follows:
 - Improving parking facilities and capacities through covered, shaded and structured parking reducing congestion, the amount of storm water and ground water contamination
 - Improving streetscapes with increased vegetation and shading, open spaces, pervious surfaces and appropriate street furniture, signage and lighting thus improving the quality of life, air quality, safety and stormwater management and reducing energy demand
 - Providing bicycle lanes and secure bicycle racks to promote alternative modes of transportation, reducing pollution and promoting a healthy life style
 - Using appropriately designed and low energy demand lighting to minimize light pollution, energy use and maintenance
 - Setting requirements for reducing potable water demand and the volume of waste water conveyance to conserve natural resources and energy while covering the demand on existing water and sewage conveyance infrastructure
 - Increasing the number of trees and amount of vegetation while reducing the amount of asphalt and impervious surfaces to mitigate heat-island effect and demand on the stormwater management infrastructure
 - Setting requirements for roof surfaces to mitigate heat island effect, increase on site stormwater retention and/or produce on site alternative energy
 - Encouraging the preservation and rehabilitation of existing buildings conserving natural resources and historic context

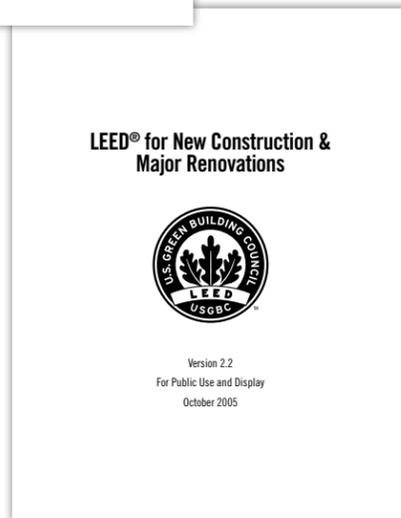
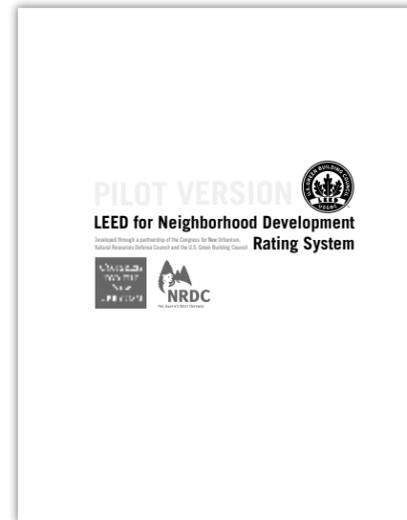
Guidelines

1. Overall Redevelopment Plan:

This Redevelopment Plan seeks to achieve “LEED-ND” (Neighborhood Development) status for Bloomfield Center. All potential developers and entities working within the Redevelopment Plan area are encouraged to work with the mayor and council to ensure overall compliance with the LEED-ND Pilot program for future certification by the township. Detailed provisions regarding the LEED-ND prerequisites and specific credits identified as attainable will be set forth in the Redeveloper’s Agreement to be negotiated.

2. Restoration & Rehabilitation:

Existing buildings being restored or rehabilitated but not meeting the USGBC’s definition for “Major Renovation” are encouraged to obtain LEED-EB (LEED Existing Building: Operations and Maintenance) certification and follow New Jersey’s Clean Energy Program.



3. Individual Development Sites:

All development projects are encouraged to meet criteria set forth in the LEED Green Building Rating System and New Jersey’s Clean Energy Program. It is up to the potential developer to determine the most appropriate LEED Rating System, for their project:

1. LEED-New Construction and Major Renovations (NC)
2. LEED-Core and Shell (CS)
3. LEED-Homes (H)
4. LEED-Retail (R)

All projects are required to meet Tier 1, as defined below. Tiers 2 and 3 are optional and qualify the projects for specific incentives.

INDIVIDUAL DEVELOPMENT SITES

<p>Tier 1</p> <p>Meet all LEED-* prerequisites including:</p> <ul style="list-style-type: none"> 1.1 LEED-NC, -CS and -R: 2 points under Energy and Atmosphere Credit 1 (mandatory under LEED- NC Version 2.2) 1.2 LEED-H: <ul style="list-style-type: none"> • Energy & Atmosphere Prerequisite 1.1: meet the performance for Energy Star Homes OR • Energy & Atmosphere Prerequisites 2.1, 3.1, 4.1, 5.1 & 6.1 • Meet LEED-NC Sustainable Sites Credit 7.2: Heat Island Effect Roofs • Meet one credit under LEED-* Water Efficiency
<p>Tier 2: optional (incentivized)</p> <ul style="list-style-type: none"> 2.1 Obtain LEED Certification 2.2 Meet one additional credit under LEED Water Efficiency
<p>Tier 3: optional (incentivized)</p> <p>Obtain LEED Silver Certification (In addition to Tier 2)</p>

4. Occupied Buildings (ongoing):

Bench Mark: LEED Existing Buildings: Operations and Maintenance (EB)

To address the ongoing performance and energy and resource consumption of occupied buildings, meeting the criteria below is strongly encouraged (for buildings occupied at least two years):

OCCUPIED BUILDINGS

<p>Tier 1: optional (incentivized)</p> <ul style="list-style-type: none"> 1.1 Meet all LEED-EB Prerequisites including the mandatory 2 points under Energy & Atmosphere Credit 1 1.2 Meet requirements for 2 credits for LEED-EB Water Efficiency Credit 2.2
<p>Tier 2: optional (incentivized)</p> <ul style="list-style-type: none"> 1.1 LEED-EB Certification 1.2 Obtain 2 credits for LEED-EB Water Efficiency Credit 2.2 1.3 Renewal of Certification during fifth year of occupation

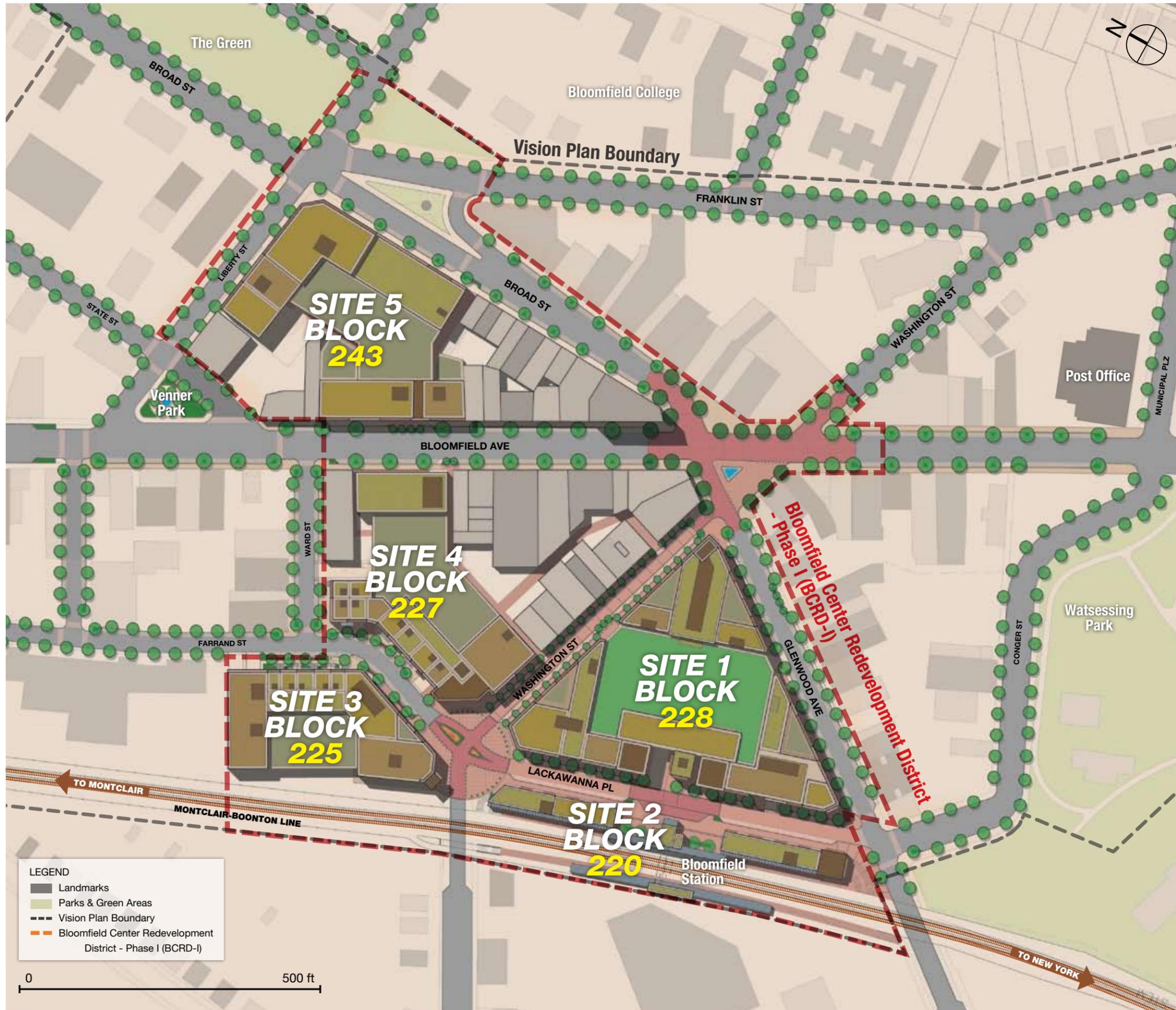
Development Sites

This section was developed to establish base development site plans that meet the objectives of this Redevelopment Plan. These development plans are conceptual in nature, and are flexible, which is demonstrated by the different options that are shown.

As the plan moves into implementation these concepts provide a head start and a base to engage in negotiations with potential redevelopers.

7.1 Overview

7.2 Development Plans



Development Sites

Overview

The Development Sites section identifies the key new construction development sites and illustrates preliminary development plan concepts. Each of these sites either includes a publicly owned property or properties where owners have expressed interest in participating in redevelopment. In some cases there are parcels owned by parties that have yet to express interest in redevelopment. The goal is to identify development strategies that optimize the value of the developments for building layout and parking.

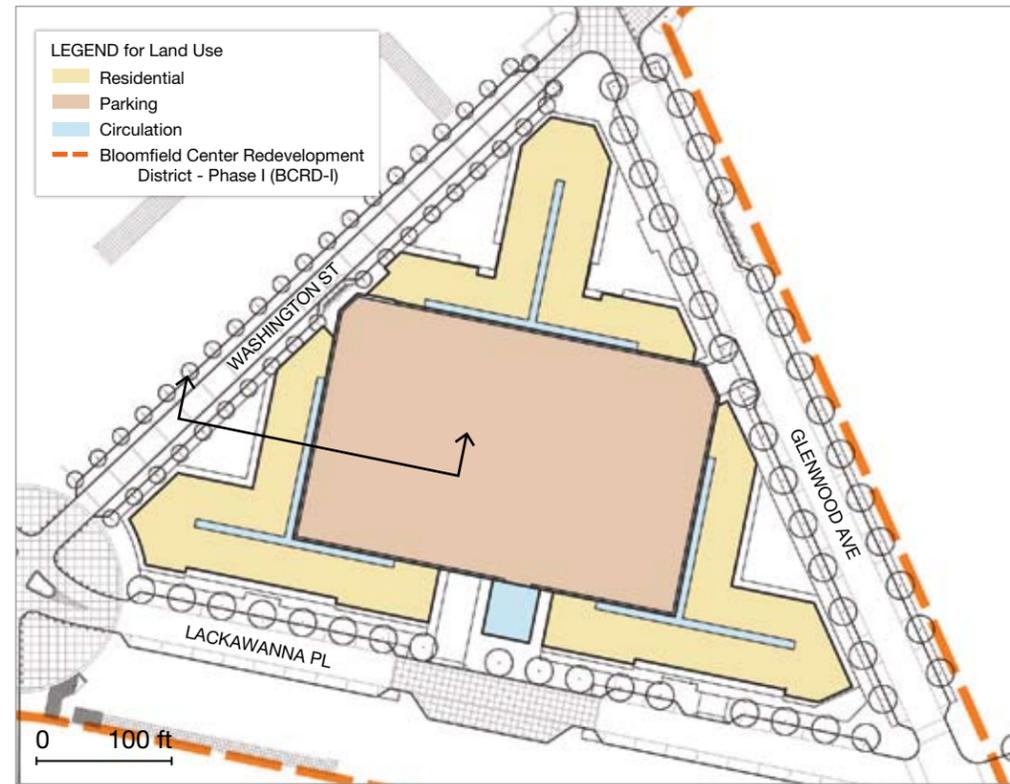
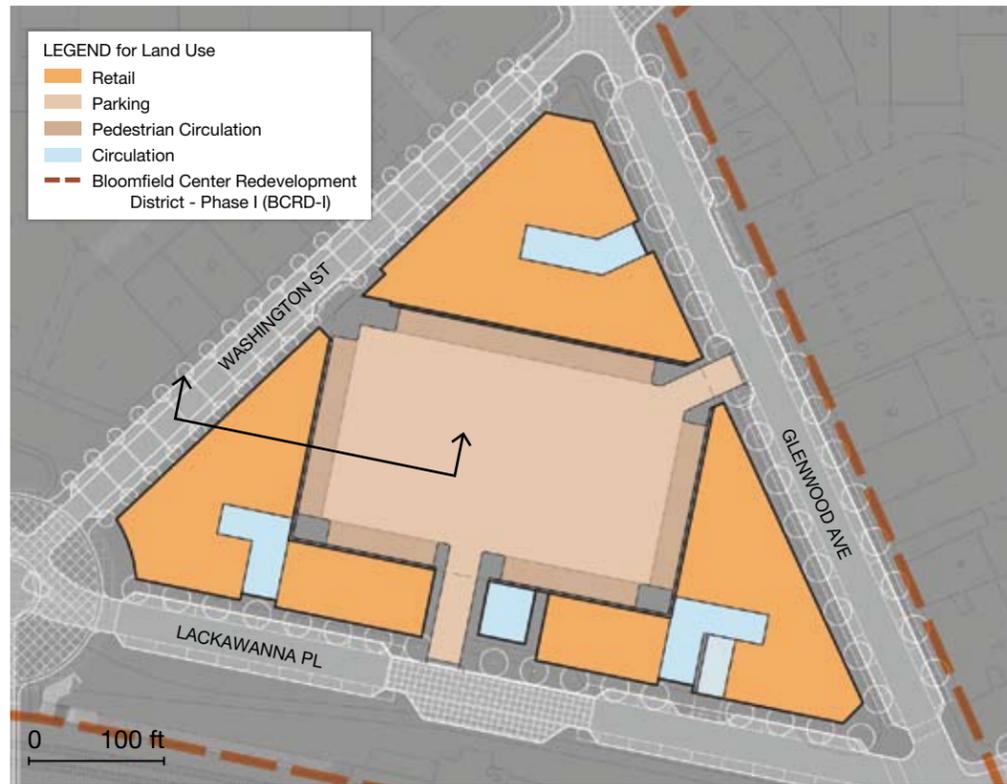
The purpose of these illustrations is to demonstrate an application of the redevelopment plan principles and guidelines. This will allow the town to have a basis for negotiating/selecting redeveloper(s).

Exhibit 17. Development Sites

Refer to Schedule A for precise location of "BCRD-I"

Overview

7.1



Site 1 (Block 228)

Due to its location between “Six Points” and the Train Station, Block 228 is a priority development site. This was reinforced by the public outreach process, where this site was chosen as the most important site for public parking and the north leg of the site along Washington Street was chosen as the “event street”, the key public space in the center of the downtown. The current state of the buildings on this block are in declining condition and many of the sites have been the subject of private acquisition for the purpose of repositioning the properties.

The site is somewhat unique due to the triangular shape. Still, it works as a fairly typical building within the redevelopment area with ground floor retail, 2nd floor either office, live/work or residential and residential above wrapping a multi-level shared parking structure.

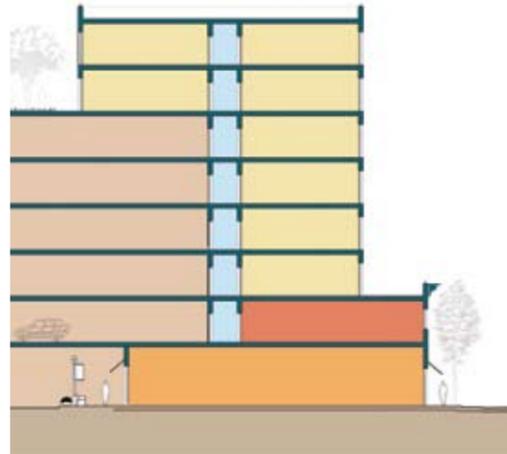
The accompanying diagrams demonstrate:

1. Typical ground floor and upper floor plans
2. Massing model images; and
3. Sections which illustrate the building’s relationship to the street

Ground Floor

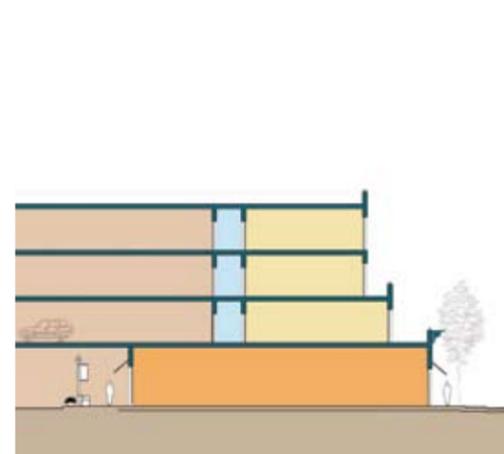
Typical Upper Floor

MASSING CONCEPT 1



Typical Section

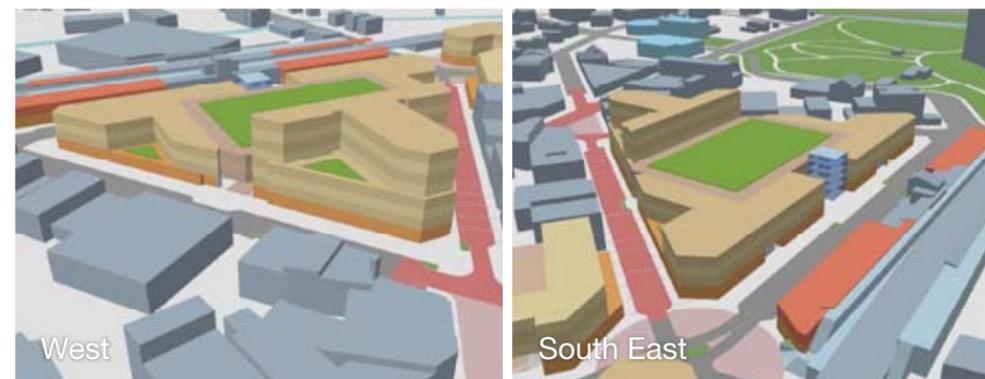
MASSING CONCEPT 2



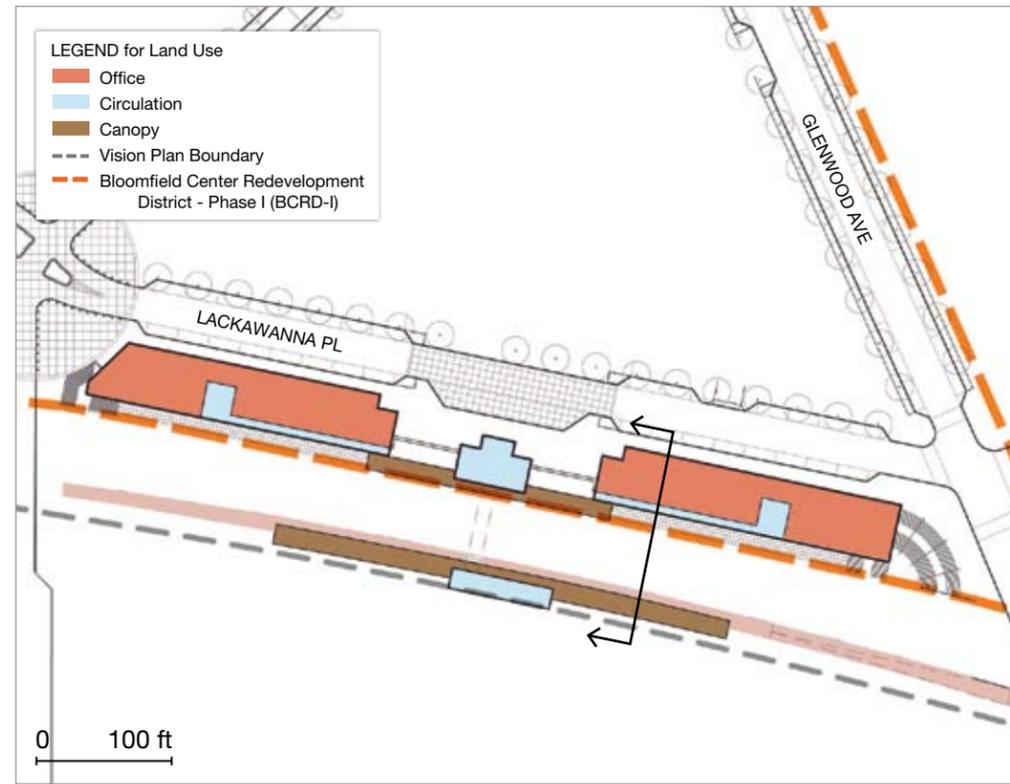
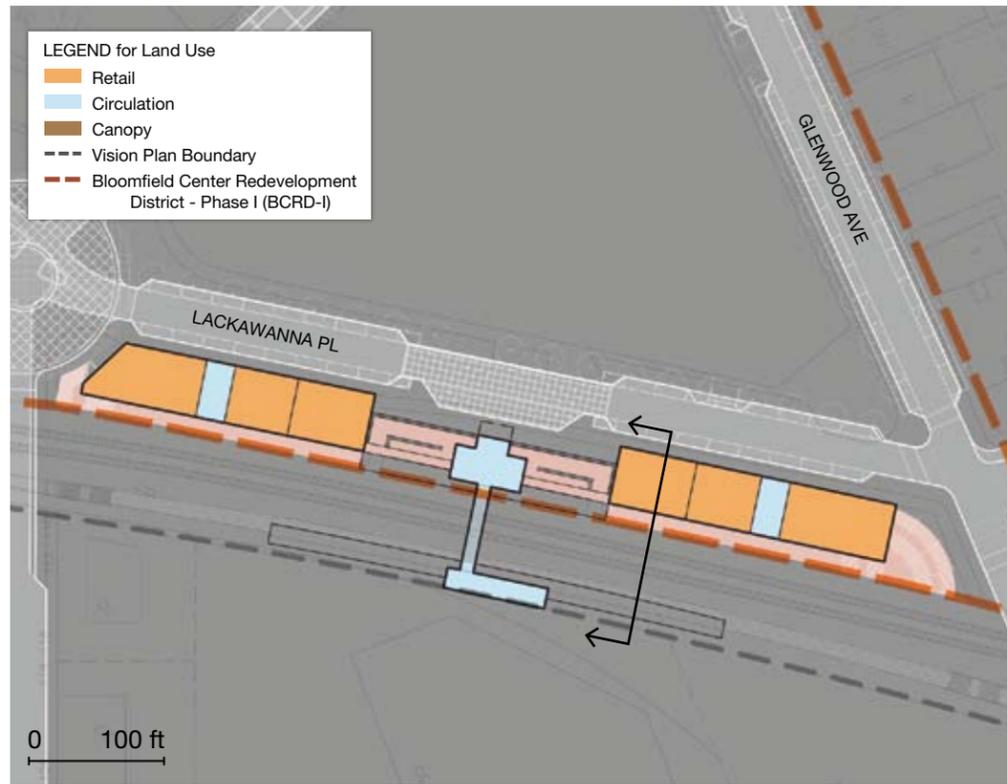
Typical Section



Massing Study



Massing Study



Site 2 (Block 220 - Lot 40)

Block 220 - Lot 40 is unique in that it encompasses the historic train station. The train station has significant public importance and is the focus of the Washington Street District. Any development or rehabilitation of this site should reinforce this image. It is also unique in its shape, being a very long, thin site which is not conventional for any type of development. Nonetheless, the site has potential as a part of this comprehensive plan but cannot be implemented without one of the adjacent blocks (228,225) moving ahead. This is due to two factors: 1. This site cannot support the parking needed for the potential uses and 2. The site lacks visibility and will need other development to happen around it to create development synergies.

The concept plan proposes ground floor retail with office. The key public improvements needed include new grand stairs at either end of the platform and improving the link under the station to the eastbound platform.

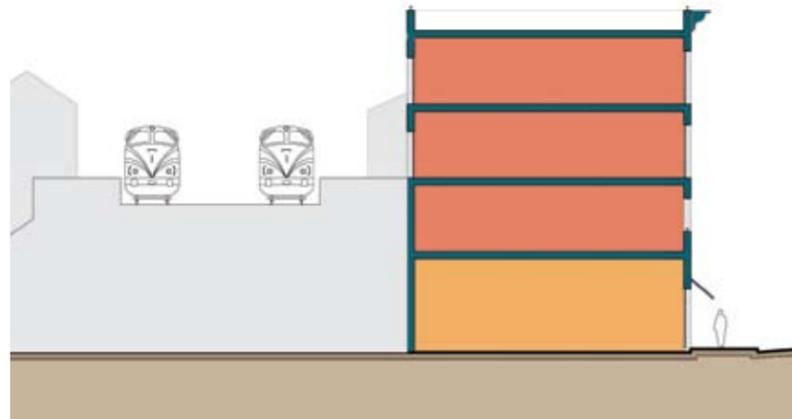
Ground Floor

Refer to Schedule A for precise location of "BCRD-I"

Typical Upper Floor

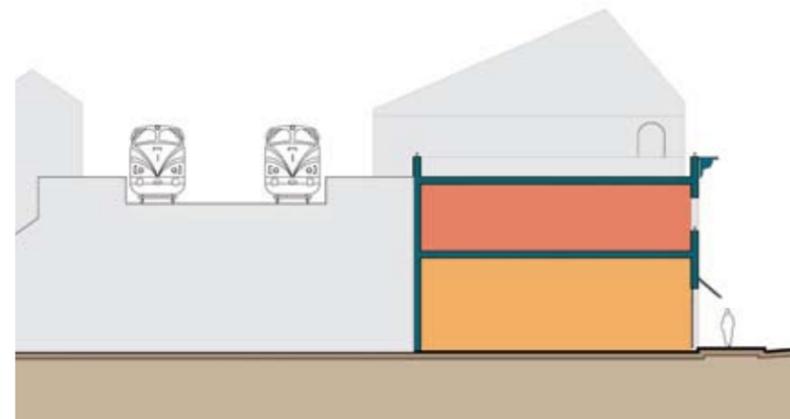
Refer to Schedule A for precise location of "BCRD-I"

MASSING CONCEPT 1

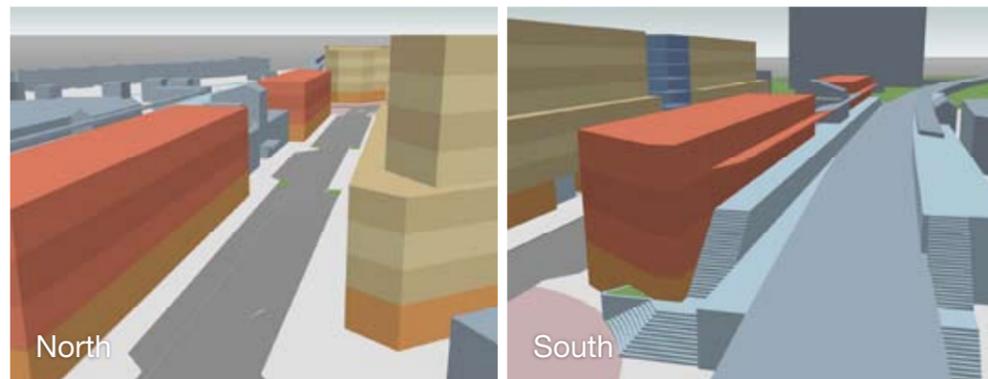


Typical Section

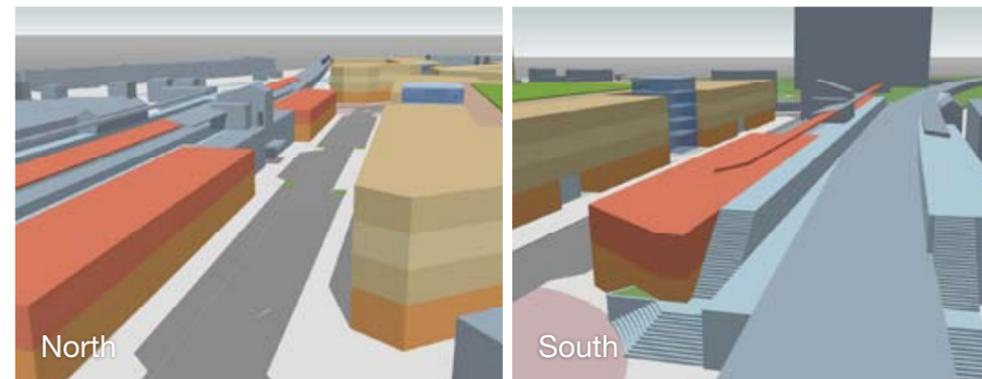
MASSING CONCEPT 2



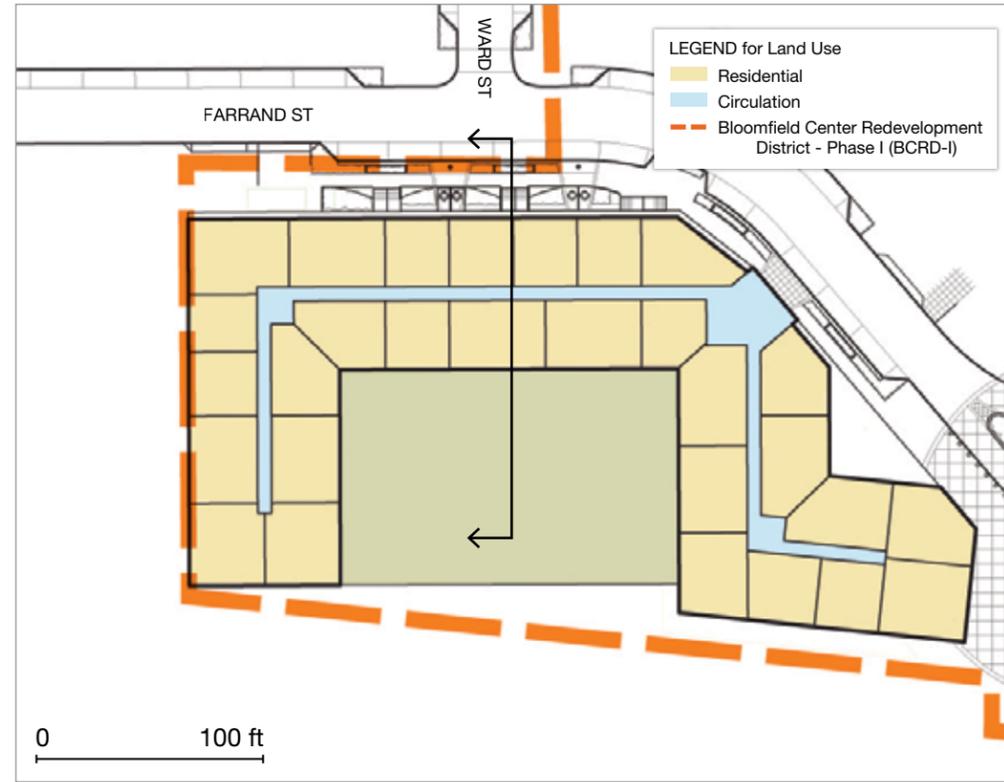
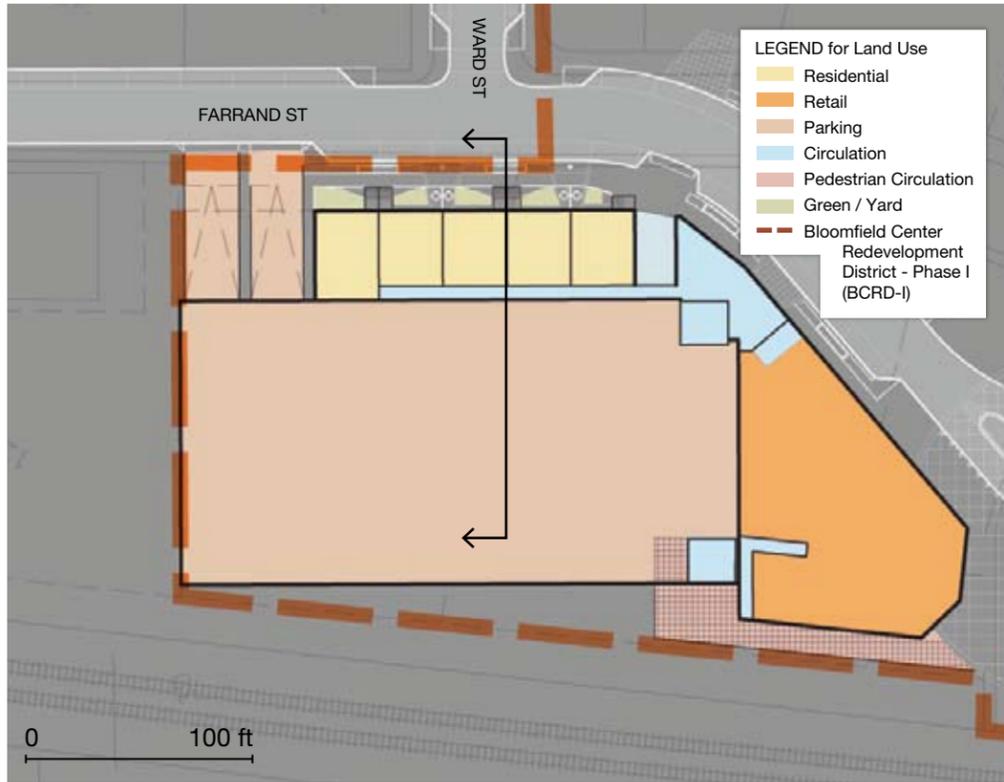
Typical Section



Massing Study



Massing Study



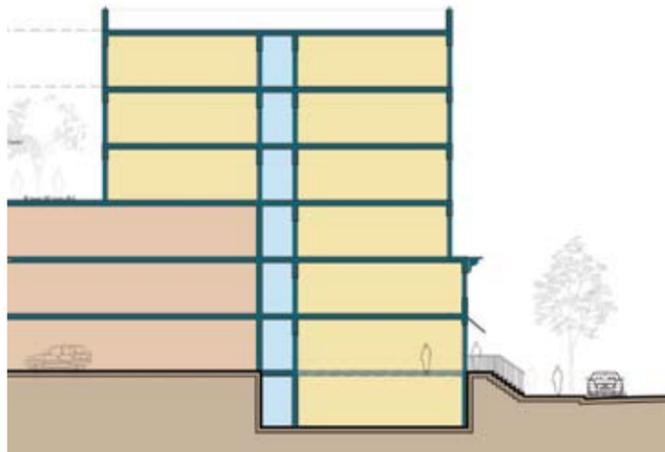
Site 3 (Block 225 - Lots 1 and 9)

The Block 225 development site is comprised of Lots 1 and 9, the current commuter parking lot along Farrand Street. These lots are owned in their entirety by the Township of Bloomfield. Uses for this site are similar to that of Block 227, which include a combination of retail focused on Washington Street and residential on the ground floor along Farrand Street. Loft-style residential units are proposed above the ground floor, building on the historic warehouse image. This site would also include public parking but is intended to be limited since the location is not as accessible as Block 227 or Block 228. Architecturally, the development plan should relate strongly to the new intersection and public space being created at the intersection of Washington Street, Farrand Street and Lackawanna Place.

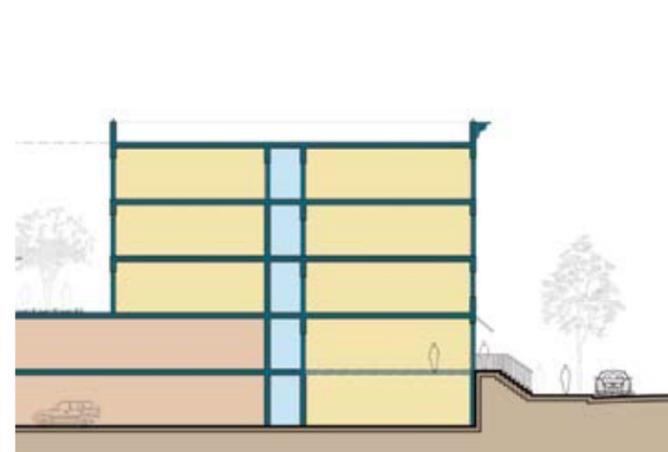
Ground Floor

Typical Upper Floor

MASSING CONCEPT 1

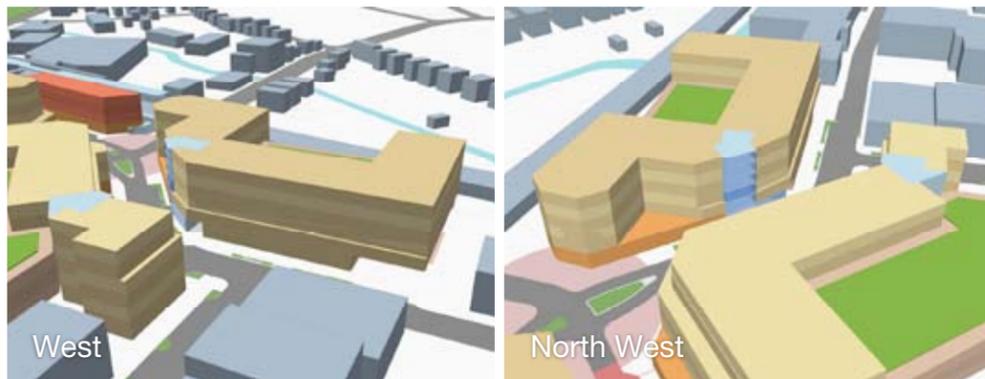


MASSING CONCEPT 2



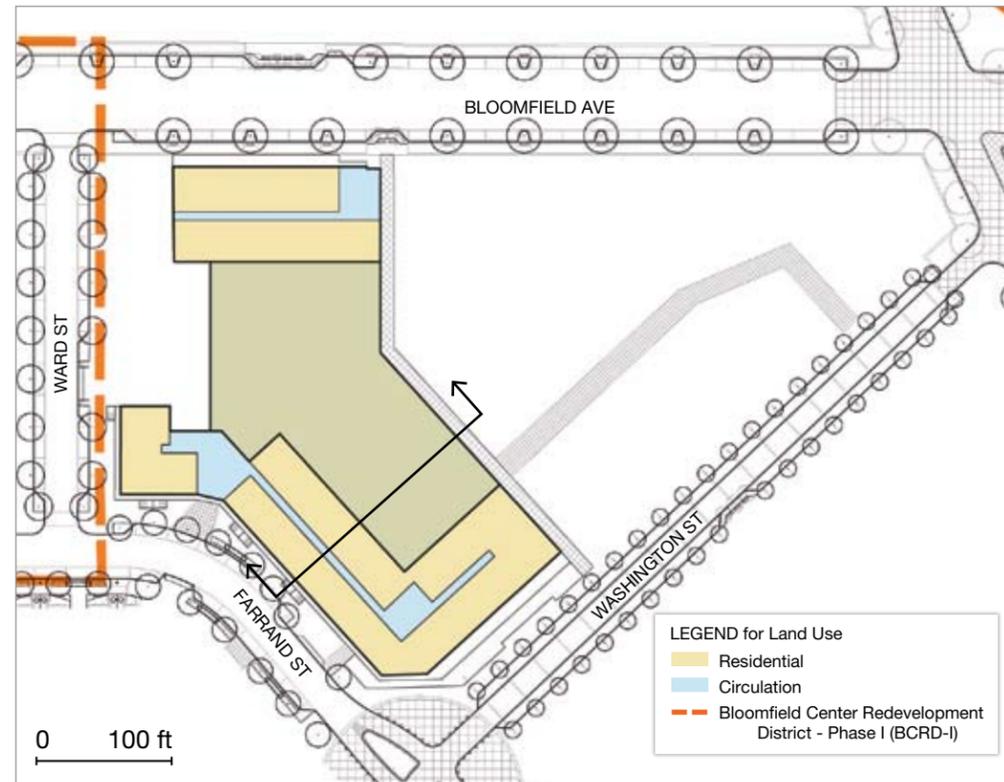
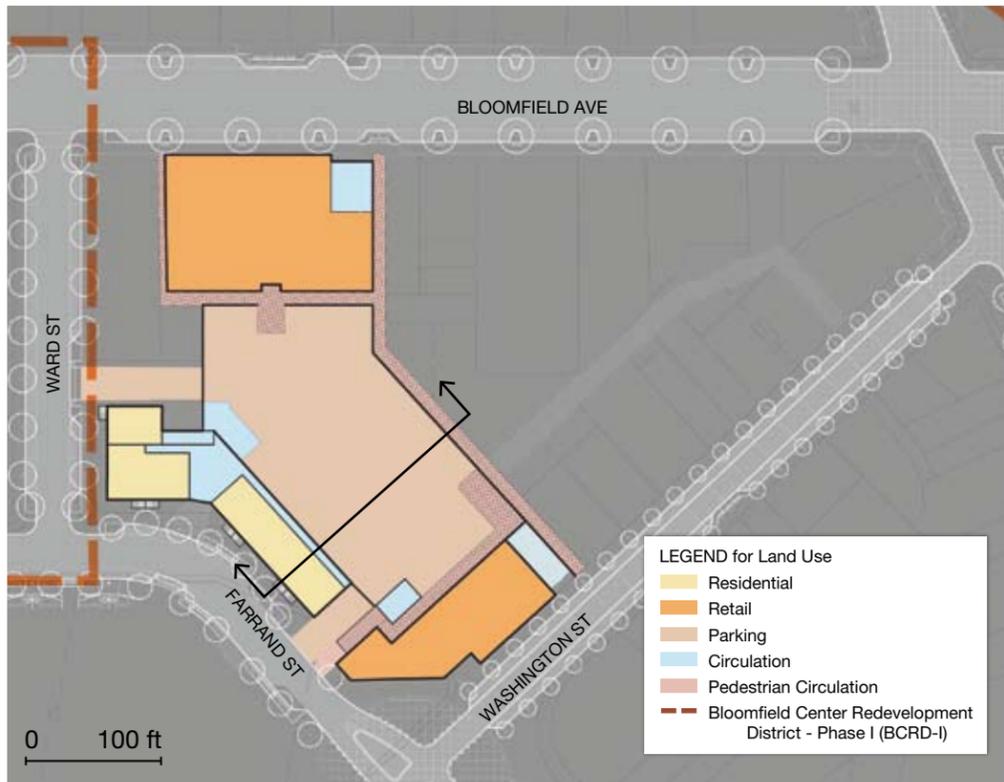
Typical Section

Typical Section



Massing Study

Massing Study



Site 4 (Block 227)

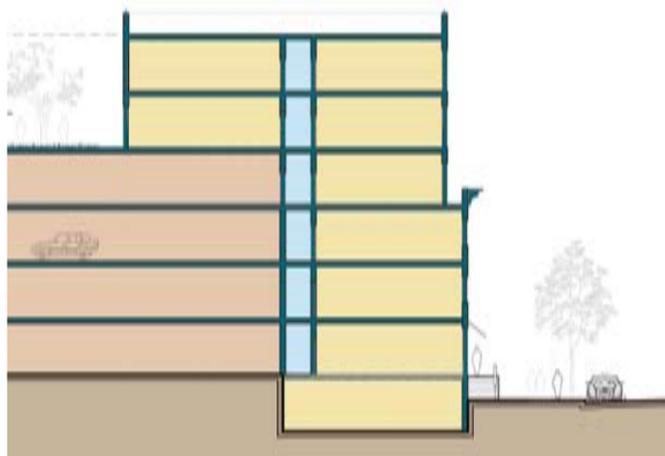
Block 227 is an important block because it links Bloomfield Avenue with Washington Street and the train station. Because of this location, the site is ideal to support public parking for retail shopping commuters. It is an infill mixed-use plan with retail fronting Bloomfield Avenue and Washington Street and residential “townhouses” with stoops along Farrand Street. Above the first floor along Farrand Street, live/work loft style residential is recommended. Along Bloomfield Avenue the upper floors could either be standard residential units or office.

This concept represents only a portion of this overall block. It is anticipated that the remainder of the block would be targeted for Preservation, Rehabilitation or Adaptive Re-use. However, this does not preclude new construction that incorporates other parcels into a Redevelopment Plan.

Ground Floor

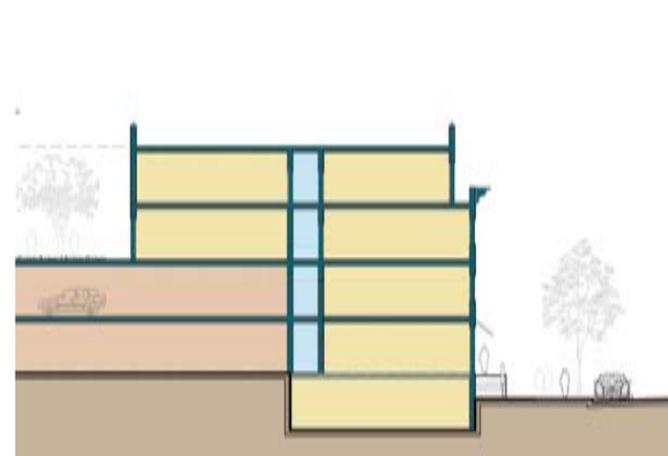
Typical Upper Floor

MASSING CONCEPT 1

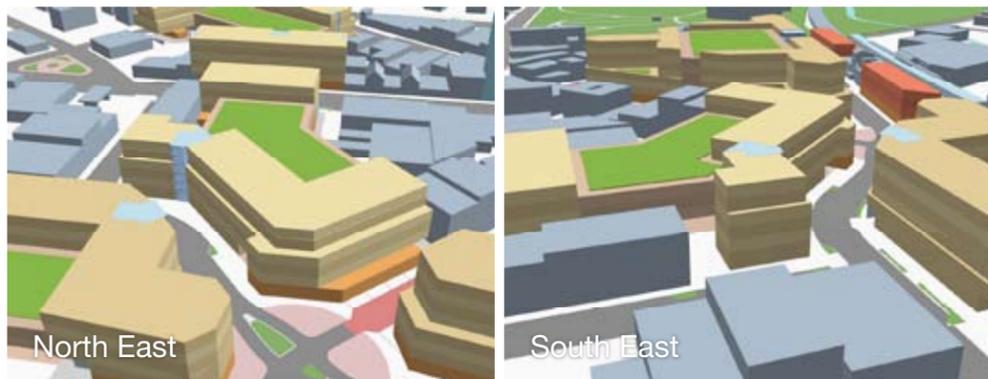


Typical Section

MASSING CONCEPT 2



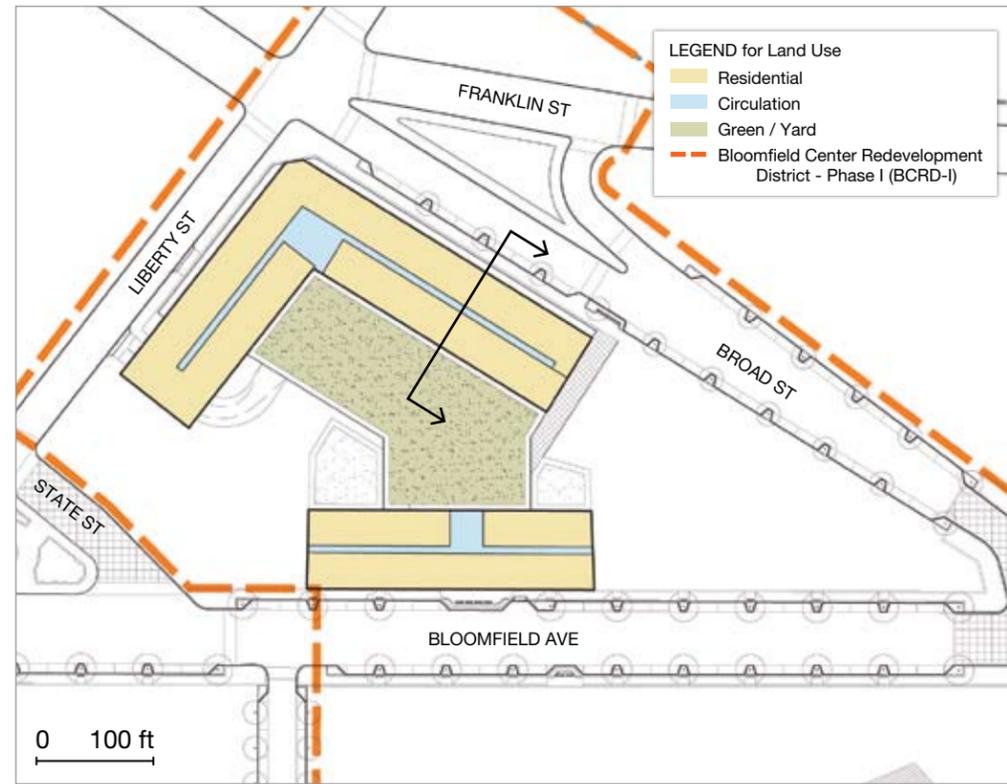
Typical Section



Massing Study



Massing Study



Site 5 (Block 243)

The Block 243 development site is located on the north side of Bloomfield Avenue and therefore has different characteristics than the other development sites. It does, like Block 227, serve as a linkage site between Bloomfield Avenue and Main Street Campus District / The Green. This concept plan does not include any public owned property. At the time of this plan, a single property owner has assembled a significant number of contiguous properties. Regardless of site control, it is critical to the overall success of this plan to create a unified development plan that links development north and south of Bloomfield Avenue.

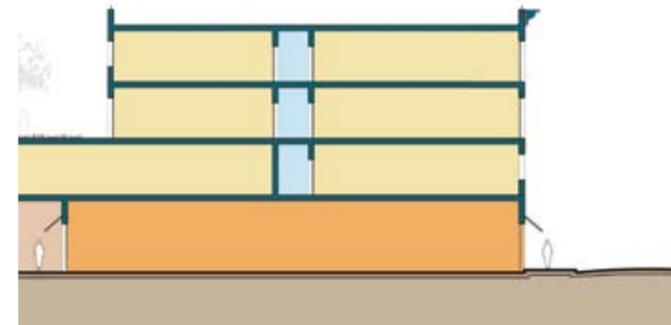
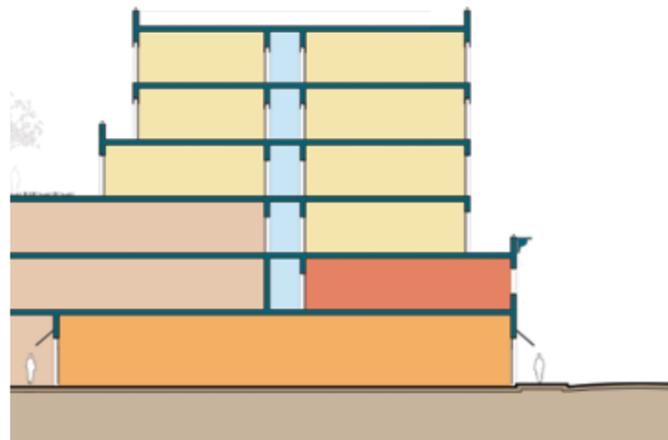
The concept as shown, assumes ground floor retail along both Broad Street and Bloomfield Avenue, public structured parking and residential on the upper floors. This block also has a range of existing buildings that are proposed for Preservation, Rehabilitation or Adaptive Re-use.

Ground Floor

Typical Upper Floor

MASSING CONCEPT 1

MASSING CONCEPT 2



Typical Section

Typical Section



Massing Study

Massing Study

Relationship to Local Objectives

This section spells out how this Redevelopment Plan relates to the general objectives of Bloomfield Township. It specifically shows how this Redevelopment Plan relates to the Bloomfield Zoning Ordinance, the regional plans and policies and the New Jersey State plans.

- 8.1 Relationship to Other Local Objectives
- 8.2 Proposed Land Uses and Building Requirements in the Project Area and Superseding Zoning
- 8.3 Relocation
- 8.4 Property to be Acquired
- 8.5 Relationship to Master Plans of Adjacent Municipalities, Essex County and the State Development & Redevelopment Plan

Relationship to Other Local Objectives

The most current statement of the local planning objectives of the Township of Bloomfield is addressed in the 2008 Bloomfield Reexamination Report. The Reexamination Report outlines these objectives as follows:

The 2003 Bloomfield Station Redevelopment Plan was framed around the following goals:

- Redevelopment of Bloomfield Station, renovate the train station, create commuter-oriented activities such as shops and restaurants.
- Create a mixed-use transit village around Bloomfield Station with new public spaces for public events, pedestrian linkages to the Center, Bloomfield College and the Town Green, and streetscape and façade improvements.
- Spur revitalization throughout Bloomfield Center, improve public perception of the area, improve pedestrian and vehicular circulation, and preserve historic character of the Center.
- Revitalize the Township, create new jobs, improve the Township's tax base, strengthen the neighborhood character, and reintegrate Bloomfield Center back into the Township.

Only the portion of the BSRP for Block, 220, Lots 26, 27, 30 and 35 is expected to proceed, and the full requirements of the BSRP will continue to govern those parcels, but much of the rest of that BSRP will be replaced by this Redevelopment Plan. The Township recently created a Parking Authority to deal with the overall need for public parking around the train station and to serve the multiple needs for parking in the area. In addition, this Plan will address and rectify the limitations imposed by the roadway configuration that evolved over the Township's history and that poses circulation challenges to the revitalization of the Downtown.

The Township has recently determined, based on an analysis from a consultant team, that it is necessary to create this new Redevelopment Plan for the entire downtown area, including all but a portion of the area comprising the Bloomfield Station Redevelopment Area, and adding areas included in the Township-wide Rehabilitation Area designation. The Mayor and Council hosted three public visioning meetings and two public presentations that were very well attended and enthusiastically supported. The Vision Presentation on April 9, 2008 was the result of these meetings and forms the basis of the requirements in this plan. Materials utilized in those meetings are available on the Township web site at: www.bloomfieldtwpnj.com.

The Re-examination Report also outlined the following continuing objectives:

- Encourage the continued development of a fully intermodal circulation system that integrates roadways, mass transit, pedestrian/bicycle routes, gre-*

enway corridors and freight and goods movement facilities. Coordinate land uses with transportation facilities to facilitate access and encourage alternatives to driving.

Under the New Jersey Transit Village initiative, the State granted "Transit Village" designation to Bloomfield in 2003. The Transit Village area extends in a one mile radius from the Bloomfield train station. As a Transit Village, Bloomfield receives State aid to revitalize the area around its train station with dense mixed-use development that encourages the use of public transit and decreases the dependence on the car.

The 2003 Bloomfield Station Redevelopment Plan began the process of making the vision of a Transit Village become a reality, but this Plan focuses the vision through the lens of community input. The public improvements that consolidate parking within structures, connects the retail and office uses of the downtown core with Bloomfield College and the Civic District and integrates higher density residential uses into the downtown are all components of this Plan that advance the Transit Village ideal.

The "Land Preservation Recommendations" of the Open Space and Recreation Plan (2004) propose four greenways, four linkages, and four hiking/biking trails all to interconnect with destination points within Bloomfield, such as existing parks, Bloomfield College, and the train station. To the extent that the "loop" will not only introduce a shuttle function to make it easier for pedestrians to circulate through the entire downtown, including the train station, without using their cars, it will also make it easier to circulate by bicycle and intersection improvements will facilitate a more comfortable crossing of Bloomfield Avenue for people on foot and on bicycles, thereby advancing the goals of the Open Space and Recreation Plan.

To reinforce the redevelopment efforts of the private sector, the Township has also been working with NJ Transit on upgrading mass transit facilities for commuters in recognition of the greatly enhanced service available through the connection of the Bloomfield Center and Watsessing rail stations to the Midtown Direct line as a result of the long awaited Montclair Connection. The Township has directed its grants consultants, Funding Group Initiatives, Inc. (FGI) and Millennium Strategies, to focus on securing funding for upgrading these two stations. To date, FGI, working with the Mayor and Township Council, has coordinated with NJ Transit to obtain commitments for a variety of improvements to both stations. These improvements include:

Bloomfield Center (Lackawanna) Station

Renovation of the Shelter and additional platform improvements through the following funding initiatives:

- 2004:
 - \$750,000 (Federal) Transportation Enhancement Grant
- 2006:
 - \$100,000 (Federal) Transportation Enhancement Grant
 - \$500,000 (Federal) FY 2006 Budget – direct appropriation via Rep. Pascrell
- 2007:
 - \$235,000 (State) Monies left over from Phase one

NJ Transit has advised the Township that it plans to rehab the shelter in cooperation with the State Historic Preservation Office (SHPO). The basic plans are as follows and are estimated to be completed in 18 months to two years:

1. The temporary roof will be replaced with one made of synthetic Spanish tiles.
2. Replacement of interior plaster walls with sheetrock, but stained to resemble the original plaster
3. Installation of wrought iron fencing around all ten windows and one door to match the current fence at the track
4. Replacement of ceiling with beaded wood to replicate the original ceiling
5. Replacement of terrazzo floor
6. Installation of wood benches throughout with heating ducts underneath
7. An area will be set aside for a concession stand.

Watsessing Station

Phase I of the renovations to Watsessing Station are estimated to take about one year to complete. Costs are estimated to be 2.5 million dollars and are expected to be covered by the NJ Transit Capital Improvement budget. Specific improvements include:

1. Historically compatible roof repairs to the inbound and outbound platform canopies on the station side of Watsessing Avenue
2. Design and installation of new canopy drainage
3. Installation of decorative railing/fence off the back side of the inbound canopy
4. Repair of all concrete walls at back of platforms
5. Installation of historically compatible canopy lighting
6. Clean up of debris west of Watsessing Avenue Bridge
7. Improved lighting in stairwell may be included by NJT for Phase I.

Phase II would begin upon securing of funding by NJT and the Township for costs estimated to be 5 to 7 million dollars and construction has been estimated to take 42 months. Phase II improvements are intended to complete the total restoration of the Station and would include such tasks as:

1. Demolition of west stairs from both platforms to Orange Street;
2. Demolition of canopies over both platforms west of Watsessing Avenue Bridge;
3. Installation of passenger communication systems;
4. Installation of decorative concrete fencing/railing at street level;
5. Re-grading of the areas off both sides of the track west of the platforms;
6. Upgrading of facility signage;
7. Clean and protect underside of Watsessing bridge canopy;
8. Repair of all concrete parapet walls at street level;
9. Installation of new sidewalks;
10. Restoration of historic circular window;
11. Repair of all concrete walls and ceilings in remaining stairs from Watsessing Avenue and Westinghouse Plaza;
12. Clean and restore masonry façade;
13. Restoration of interior building to accommodate new passenger waiting area if funds can be raised and the existing tenant can be accommodated.

- Promote the growth of a diversified economic base that generates employment growth, increases tax rates, improves income levels and facilitates the redevelopment of Brownfield sites. Focus economic development efforts on growth sectors of the economy and existing commercial and industrial districts.*

The Bloomfield Center Alliance, Inc. (BCA), a non-profit organization, is the management entity for the Bloomfield Center Special Improvement District (SID). The SID was created in 2001, and comprises over 200 residential and commercial properties. The commercial property owners are assessed an additional 6 percent over their regular municipal taxes to fund capital programs, marketing and operations that enhance and revitalize the SID. The BCA is made up of property and business owners, residents, Bloomfield College, and Township representatives.

The successful implementation of this Plan by both redevelopers and existing property owners will provide a shot of adrenalin to the vitality of Bloomfield's downtown, infusing a richer variety of residential unit types to enliven the street both day and night, weekday and weekend. Such a revitalization will strengthen the Township's economy and propel the BCA to larger projects and more ambitious undertakings.

**Relationship
to Local
Objectives**

8.1

c. **Maintain the existing system of parks, recreation and open space and explore the development of additional facilities to meet the needs of Township residents. Advocate for increased maintenance and improvements in Essex County parks.**

In November 2001, the Township voters approved a dedicated open space tax of one cent (\$0.01) per one hundred dollars (\$100.00) assessed value to provide a steady source of funding to acquire property or easements in the Township for the purposes of active recreation, passive recreation, and water quality protection. An Open Space Trust Fund Committee was created in 2003 to advise the Mayor and Council on future acquisitions. In August 2004, the Open Space Trust Fund Committee in partnership with the Morris Land Conservancy compiled an Open Space and Recreation Plan (OSRP) to serve as a guide for the acquisition and maintenance of land for open space and recreational purposes, and increase the Township's resident's quality of life and maintain Bloomfield as a desirable community.

While preservation of open space is not normally a part of a redevelopment plan for a downtown, the enhancement of Watsessing Park and creation of new public open spaces, in the form of plazas, small sitting areas and pocket parks, are important additions to the system of parks and open space within the Township.

Proposed Land Uses and Building Requirements in the Project Area and Superseding Zoning

The proposed land uses and building requirements in the project area are set forth in Sections 3 through 6 of this Redevelopment Plan. The zoning for the properties identified in this Redevelopment Plan as Areas in Need of Redevelopment and Areas In Need of Rehabilitation are superseded by the land uses identified in this Bloomfield Center Redevelopment Plan - Phase I, and all development and redevelopment for said properties shall be governed by the land use provisions and design guidelines contained herein. The zoning district map is amended upon the adoption of this Redevelopment Plan to include the superseding provisions of this Redevelopment Plan.

Relocation

To the extent required by law and in the event that residents or businesses in the Redevelopment Area are displaced as a result of the acquisition of property, adequate provision for the temporary and permanent

relocation of such residents or businesses, as necessary, shall be made in accordance with all applicable regulations. Prior to the undertaking of any voluntary acquisition of property by the designated redeveloper(s), to the extent required by law, a Workable Relocation Assistance Plan shall be submitted to and approved by the Department of Community Affairs in accordance with the requirements of N.J.A.C. 5:11-1.1 et. seq. It is estimated that sufficient decent, safe and sanitary dwelling units affordable to displaced residents will be available to them in the existing local housing market.

Property to be Acquired

All property situated in the Redevelopment Area and the Rehabilitation Area as identified in this Redevelopment Plan may be acquired through voluntary negotiation only. The Mayor and Council may sell, lease or otherwise convey to a designated redeveloper(s), subject to restrictions, controls and requirements of this Redevelopment Plan, all or any of the properties that it owns or may acquire.

Eminent Domain will not be utilized under the enabling provision of the Local Redevelopment and Housing Law, N.J.S.A. 40A:12A-8. However, to achieve a coordinated solution to the need for public parking to support the redevelopment and rehabilitation contemplated within the BCRD-I it may be determined by the Township Council, acting as the Redevelopment Entity, to enter into a Redeveloper Agreement with the Bloomfield Parking Authority to implement portions of the Bloomfield Center Redevelopment Plan - Phase I in accordance with its statutory powers and to acquire property for such purposes and to provide one or more public parking facilities on any block within the BCRD-I in accordance with this Redevelopment Plan. Should the Parking Authority not be able to acquire private property needed for such projects through a negotiated purchase, the Mayor and Council has the authority in its discretion to grant the power of Eminent Domain to the Bloomfield Parking Authority pursuant to the Parking Authority Law, N.J.S.A. 40:11A-1 et. seq., separate and apart from the authority granted in Section 8 of the Local Redevelopment and Housing Law.

Pursuant to N.J.S.A. 40A:12A-15, Eminent Domain is not legally permitted in furtherance of a redevelopment plan for an area in need of rehabilitation. Additionally, the Township determines not to utilize Eminent Domain as authorized in the enabling provision of the Local Redevelopment and Housing Law to acquire property situated in the areas in need of redevelopment that are subject to this Redevelopment Plan. However, this provision shall not be interpreted to limit the use of Eminent Domain for the acquisition of property by a public body or any interest therein by a public body for any other

public purpose authorized by law such as streets, public utilities, parks, parking facilities, etc.

Relationship to Master Plans of Adjacent Municipalities, Essex County and the State Development & Redevelopment Plan

Plans of Adjacent Municipalities

The Township of Bloomfield is located on the northern border of Essex County. The adjoining municipalities are Belleville Township, Clifton City, East Orange City, Glen Ridge Borough, Montclair Township, Newark City, and Nutley Township.

- Glen Ridge Borough**
 The BCRD-I is adjacent to the Borough of Glen Ridge, which is linked to Bloomfield's downtown by Bloomfield Avenue. As Glen Ridge is primarily a residential community situated between the downtowns of Montclair and Bloomfield along the Bloomfield Avenue corridor, this Redevelopment Plan can be expected to provide new services and products resulting from the Redevelopment Plan that will be available to residents of Glen Ridge, many of whom can be expected to utilize Bloomfield Station. As can be seen in the comparison of the Zoning Maps of the Borough of Glen Ridge and Township of Bloomfield, the two municipalities share a common boundary that runs southwest to northeast. Residential neighborhoods along that boundary are consistent and almost indistinguishable from one side of the municipal boundary to the other. Along Bloomfield Avenue, the B1 Zone in Bloomfield is consistent with the B Zone and OS-H (Open Space Historic) Zones in Glen Ridge. As this Redevelopment Plan is intended to diversify and strengthen the downtown and supplement the B1 Zoning with consistent business, retail, office, residential and mixed uses, this Plan is consistent with the Zone Plan of the Borough of Glen Ridge.
- Belleville Township**
 Belleville Township borders Bloomfield Township along the southern half of Bloomfield's eastern border, from Hoover Avenue in the north to Bloomfield Avenue to the south. The 2002 Master Plan found that "the land use and zoning pattern along the municipal border is consistent with minor exceptions where the redevelopment of former industrial sites has occurred". The Belleville border is a significant distance from the BCRD-I and the most direct vehicular link is via Franklin Street and Newark Avenue on the south side of the Garden State Parkway. The Bloomfield Center Redevelopment Plan - Phase I will therefore not materially change the consistency with the zone plan for Belleville Township.

- Clifton City**
 The City of Clifton borders Bloomfield Township to the north. The 2002 Master Plan states that "the land use and zoning pattern along the municipal border is consistent with the exception of the area west of the Garden State Parkway. In this area, the land use and zoning for properties fronting on Broad Street in Bloomfield is residential while in Clifton it is commercial." The BCRD-I is at the southwestern edge of Bloomfield Township, while Clifton is at the extreme northeasterly border, with several exits of the Garden State Parkway in between. For those reasons, the Bloomfield Center Redevelopment Plan - Phase I will therefore not materially change the consistency with the zone plan for the City of Clifton.
- East Orange City**
 The City of East Orange shares a boundary along the extreme southerly boundary of Bloomfield Township. The 2002 Master Plan describes the common boundary between the two municipalities as follows: "The land use and zoning pattern along the municipal border is consistent reflecting a common development history and neighborhood compatibility. The major transitional area is in the section of the Township consisting of Watsessing Center and the former Westinghouse site". The Westinghouse site was recently rezoned from M-1 (manufacturing) to Commuter Oriented Residential District (CORD) in anticipation of its redevelopment as a Transit Oriented Development (TOD) within close proximity to the Watsessing Train Station. This rezoning is consistent with the Transit Village designation and TOD emphasis of the Bloomfield Center Redevelopment Plan - Phase I.
- Montclair Township**
 The majority of Bloomfield's westerly border is shared with the Township of Montclair. The 2002 Master Plan describes the area along the border as being "characterized by a majority of residential uses with parks and open space". Single family neighborhoods span the border, as well as a shared County park. Because the BCRD-I is to the south of the common border with Montclair and is separated from Montclair by the Borough of Glen Ridge, the Bloomfield Center Redevelopment Plan - Phase I is consistent with the zone plan of the Township of Montclair.
- Newark City**
 The City of Newark shares a relatively small border with Bloomfield Township at the southeast corner, very near the Belleville town line. The 2002 Master Plan describes the area along the border with Newark as the area between Bloomfield Avenue to the north and First Avenue to the south, which "is characterized by a mix of residential and commercial

Relationship to Local Objectives

8.2-5

uses with neighborhood retail/services on Bloomfield Avenue and detached one and two-family housing to the south”. Bloomfield Avenue is the common corridor linking the two towns, ultimately leading to the BCRD-I. However, because even the Vision Plan boundaries extend only to the Garden State Parkway and the redevelopment plan boundaries are smaller still, the consistency between the Bloomfield Center Redevelopment Plan and the zone plan for the City of Newark will remain unchanged from the 2002 Master Plan.

- Nutley Township

The Township of Nutley borders Bloomfield along the northern half of its easterly boundary, from the Belleville line at Martin Street/East Passaic Avenue north to the Clifton line. The 2002 Master Plan describes the common border as an area “characterized by a majority of residential uses with scattered commercial uses on East and West Passaic Avenues. The land use and zoning pattern along the municipal border is consistent and no changes are recommended.” As there are no direct connections between East Passaic Avenue and the BCRD-I (East Passaic runs south parallel to the Nutley border, but then turns west to its junction with JFK Boulevard, which then runs south again to the Central Business District), the BCRP-I will not change the findings of consistency made in the 2002 Master Plan.

Essex County Cross-Acceptance Report

Because the Essex County Master Plan has not been updated in many years, the 2004 Essex County Cross-Acceptance Report was used as a surrogate for the County Master Plan because it is relatively current, comprehensive in scope and addresses numerous municipal and county planning issues. This Redevelopment Plan incorporates many of the key findings and recommendations of the County Cross-Acceptance Report, as follows:

- Redevelopment and Revitalization

The Redevelopment Plan supports the continued redevelopment and revitalization of Bloomfield.

- Transportation

The Redevelopment Plan promotes alternative transportation methods with provisions for a shuttle and/or bus service within the redevelopment area to connect with the township’s existing mass transit network.

- Aging Infrastructure

The Redevelopment Plan will provide for utility and infrastructure upgrades necessary to support the redevelopment, especially the need for structured parking solutions for downtown Bloomfield in the area around Bloomfield Station.

New Jersey State Development & Redevelopment Plan

The State Development and Redevelopment Plan places the Township of Bloomfield within the Metropolitan Planning Area (PA-1). The Redevelopment Plan is compatible with the State Plan’s intention for the Metropolitan Planning Area, which is to provide for much of the state’s future redevelopment; revitalize cities and towns; promote growth in compact forms; stabilize older suburbs; redesign areas of sprawl; and protect the character of existing stable communities. The redevelopment plan also furthers the following State Plan policy objectives:

- Land Use

Ensure efficient and beneficial utilization of scarce land to strengthen its existing diversified and compact nature.

- Housing

Provide a full range of housing choices through redevelopment, new construction, rehabilitation, adaptive reuse and the introduction of new housing into appropriate nonresidential settings.

- Economic Development

Promote economic development by encouraging strategic land assembly, site preparation and infill development, public/private partnerships and infrastructure improvements.

- Redevelopment

Encourage redevelopment at intensities sufficient to support transit, a broad range of uses and efficient use of infrastructure. Promote design that enhances public safety, encourages pedestrian activity and reduces dependency on the automobile.

- Public Facilities and Services

Complete, repair or replace existing infrastructure systems to eliminate deficiencies and provide capacity for sustainable development and redevelopment.

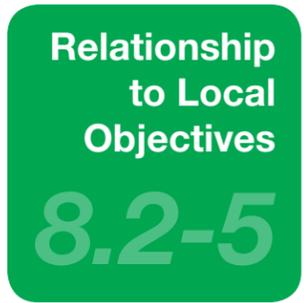


Exhibit 17. Zoning Map of Borough of Glen Ridge, NJ

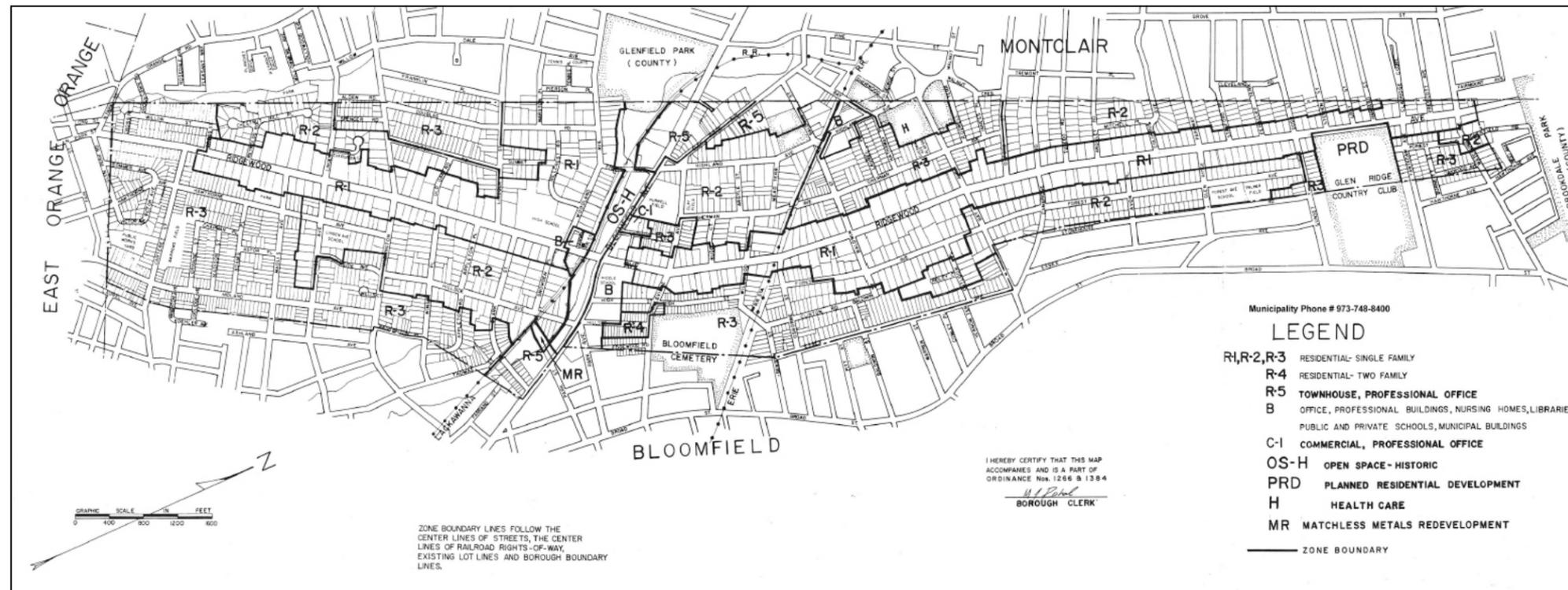
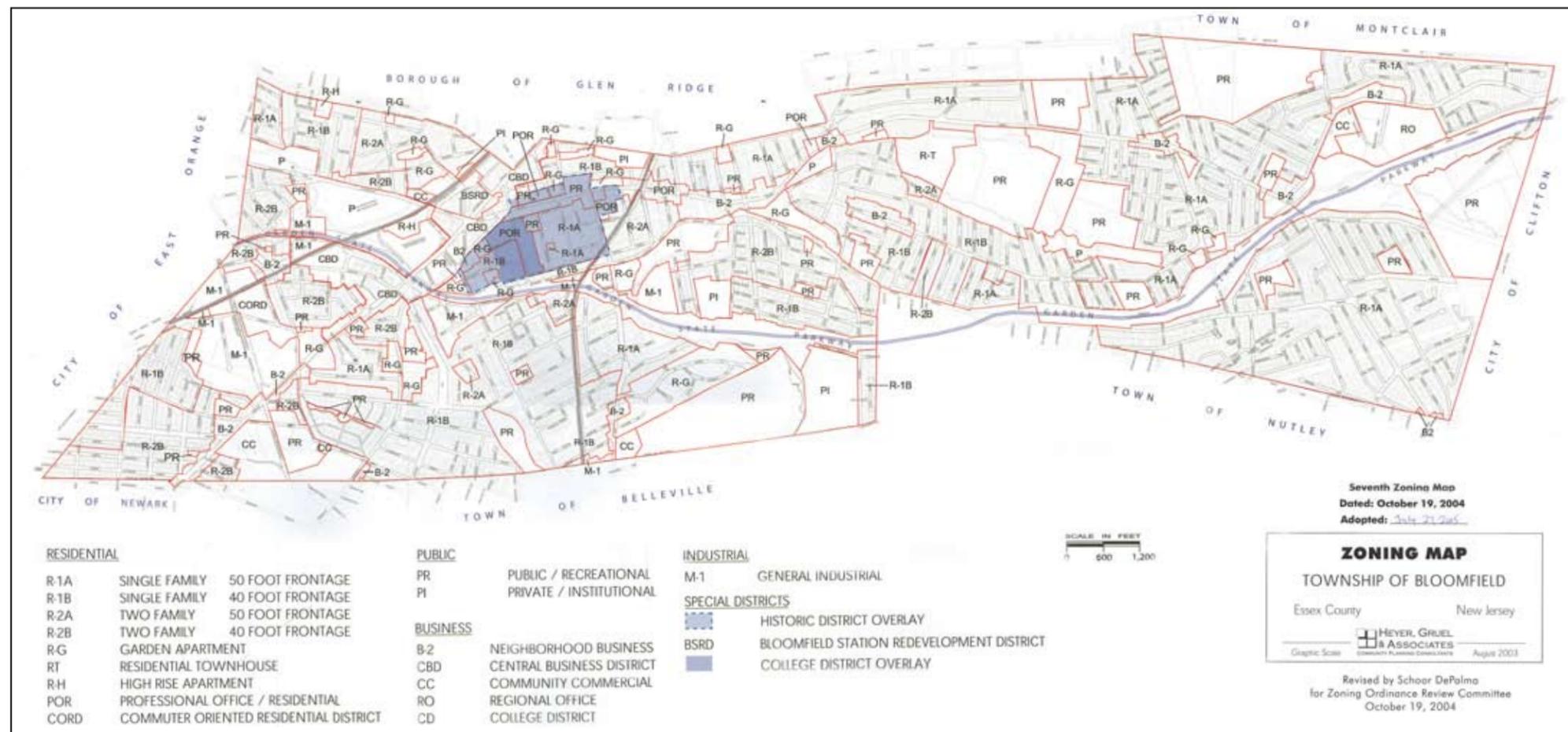


Exhibit 18. Zoning Map of Borough of Bloomfield, NJ



Relationship to Local Objectives

8.1-5

Redevelopment Plan Organization

This section lays out the role of the town and process for moving the redevelopment into implementation.

- 9.1 Role of Township
- 9.2 Redeveloper Selection
- 9.3 Affordable Housing Requirement
- 9.4 Public Financing and Incentives
- 9.5 Project Approvals Process

Role of Township

The Township of Bloomfield, through the Mayor and Council acting as the Redevelopment Entity will be solely responsible for the implementation of the “BCRP-I”. All redeveloper agreements will be executed through authorization of the Mayor and Council.

Redeveloper Selection

The Mayor and Council may in its discretion make interim redeveloper selections, subject to the negotiation and execution of an interim redevelopment agreement, and require the interim redeveloper to establish a separate escrow to reimburse the Township for all expenses in connection therewith.

In order to assure that the vision of the “BCRP-I” will be successfully implemented in an effective and timely way to promptly achieve the public purpose goals of the Redevelopment Plan, the Mayor and Council, acting as the Redevelopment Entity, will designate the redeveloper(s) for any redevelopment/rehabilitation project. All redeveloper(s) will be required to execute a redevelopment agreement satisfactory to the Mayor and Council as one of the requirements to be designated as the redeveloper(s).

It is anticipated that the implementation of this Redevelopment Plan will utilize a process for the competitive selection of one or more redeveloper(s).

The intent of this section of the Plan is to set forth the procedural standards to guide redeveloper selection. The Mayor and Council, acting as the Redevelopment Entity may, at any time, proactively solicit potential redevelopers by utilizing appropriate methods of advertisement and other forms of communication, or may, in its discretion, entertain an unsolicited proposal from a prospective redeveloper(s) for redevelopment of one or more redevelopment parcels.

Since this Redevelopment Plan includes lands of an Area in Need of Rehabilitation, the present owners of property within the “BCRD-I” may be given an opportunity to participate in the redevelopment program through the reinvestment, rehabilitation, and/or redevelopment of their properties in accordance with the land uses, building and design requirements of this Redevelopment Plan. To that end, the present property owners of properties within the “BCRD-I” are encouraged to present their own proposals for redevelopment in accordance with this Plan. Each owner not in the area in need of redevelopment, but within the area in need of rehabilitation as set forth in this Redevelopment Plan, shall have the opportunity to become the redeveloper for their property, provided that all requirements have been abided by, and approvals of submitted applications have been granted.

The selection of a redeveloper by the Mayor and Council, the Township of Bloomfield’s Redevelopment Entity, may be based on a competitive selection process. Under a competitive selection process, which may be undertaken from time to time in the discretion of the Mayor and Council, an applicant for selection as a redeveloper will be required to submit materials to the Mayor and Council that specify their qualifications, financial resources, experience and design approach to the property in question. The competitive selection process will likely include the submission of some or all of the following materials (additional submission materials may be requested by the Mayor and Council as deemed appropriate to the lands in question):

- Conceptual plans and elevations sufficient in scope to demonstrate that the design approach, architectural concepts, sustainability guidelines, number and type of dwelling units, parking, traffic circulation, landscaping, recreation space and other elements are consistent with the objectives and standards of this Redevelopment Plan.
- Anticipated construction schedule, including estimated pre-construction time period to secure permits and approvals
- Documentation evidencing the financial responsibility and capability with respect to carrying out the proposed redevelopment and/or rehabilitation including but not limited to: type of company or partnership, disclosure of ownership interest, list of comparable projects successfully completed, list of references with name, address and phone information, list of any general or limited partners, and financial profile of the redeveloper entity.
- If land acquisition is contemplated or necessary, specific identification of land requiring acquisition. The estimated offering price and status of negotiation to purchase the lands in question should be provided.

The following provisions regarding redevelopment are hereby included in connection with the implementation of this Redevelopment Plan and the selection of a redeveloper(s) for any property or properties included in the Redevelopment Plan and shall apply notwithstanding the provisions of any zoning or building ordinance or other regulations to the contrary:

1. The redeveloper will be obligated to carry out the specified improvements in accordance with the Redevelopment Plan.
2. The redeveloper, its successor or assigns shall develop the property in accordance with the uses and building requirements specified in the Redevelopment Plan.
3. Until the required improvements are completed and a Certificate of Completion is issued by the Redevelopment Entity, the redeveloper covenants provided

for in N.J.S.A. 40A:12A-9 and imposed in any redeveloper agreement, lease, deed or other instrument shall remain in full force and effect.

4. The redevelopment agreement(s) shall include a provision that requires the redeveloper to provide for all affordable housing that results from the calculation of the “Growth Share” generated by its project, in compliance with the then applicable Rules and Regulations of the Council on Affordable Housing (COAH), or as otherwise provided by law.
5. The redevelopment agreement(s) shall contain provisions to assure the timely construction of the redevelopment project, the qualifications, financial capability and financial guarantees of the redeveloper(s) and any other provisions to assure the successful completion of the project.
6. The designated redeveloper(s) shall be responsible for any installation or upgrade of infrastructure related to their project whether on-site or off-site. Infrastructure items include, but are not limited to gas, electric, water, sanitary and storm sewers, telecommunications, recreation or open space, streets, curbs, sidewalks, street lighting and street trees or other improvements. The extent of the designated redeveloper’s responsibility will be outlined in the redeveloper’s agreements with the Township. All utilities shall be placed underground.
7. The designated redeveloper will be responsible for all costs and fees related to compliance with and verification of sustainability guidelines and LEED certification.
8. All infrastructure improvements shall comply with applicable local, state and federal law and regulations, including the Americans with Disabilities Act And the Prevailing Wage Law, where applicable.
9. In addition to the provision of the infrastructure items set forth herein, the Redevelopment Agreement may provide that the redeveloper(s) will agree to provide amenities, benefits, fees and payments in addition to those authorized under the Municipal Land Use Law.

Affordable Housing Requirements

Assembly Bill No. A500 (“A500”) passed both Houses of the New Jersey Legislature on June 23, 2008, and was signed into law by Governor Corzine on July 17, 2008. A500 revises the laws concerning the provision of affordable housing and the requirements of a redevelopment plan. More specifically, A500 requires that a redevelopment plan include (i) as of the date of the adoption of the resolution finding the area to be in need of redevelopment, an inventory of all affordable housing units that are to be removed as a result of implementation of the

redevelopment plan; and (ii) a plan for the provision, through new construction or substantial rehabilitation of one comparable, affordable replacement housing unit for each affordable housing unit that has been occupied at any time within the last 18 months, that is subject to affordability controls and that is identified as to be removed as a result of implementation of the redevelopment plan.

The following subsections (1) and (2) satisfy the affordable housing “inventory” and “replacement unit plan” requirements of this law.

(1) Inventory of Affordable Housing

As of August 7, 2006, which is the date the Mayor and Council of the Township of Bloomfield duly adopted a Resolution finding Bloomfield Center to be an area in need of redevelopment; table 4 identifies all housing units affordable to low and moderate income households, that are to be removed as a result of implementation of this Redevelopment Plan.

The Bloomfield Center Redevelopment District - Phase I consists of 51 total rental units. We were able to obtain rental information from 26 of those units. As a result of our survey none of the properties are subject to affordable controls. However, all 26 rents surveyed in our study would be affordable to low/moderate income households in Essex County. The following table cites the affordability rent level calculations in Essex County as of 2006.

Table 4. Affordability Rent Level

Maximum Allowable Gross Rents in Essex County 2006

50% Max. Rents		60% Max. Rents		80% Max. Rents	
1 BR	\$781	1 BR	\$949	1 BR	\$1,117
2 BR	\$950	2 BR	\$1,140	2 BR	\$1,341
3 BR	\$1,097	3 BR	\$1,317	3 BR	\$1,549
4 BR	\$1,223	4 BR	\$1,468	4 BR	\$1,728

(2) Plan for the Provision of Affordable Replacement Housing

The plan for the provision of one comparable, affordable replacement housing unit for each affordable housing unit that has been occupied at any time within the last 18 months, that is subject to affordability controls and that is identified as to be removed as a result of implementation of this Redevelopment Plan (the “replacement units”) shall include development of new projects at the following selected sites, which are in close proximity to the BCRD-I and within the Township of Bloomfield. Since the units located in the BCRD-I are not subject to affordable controls, no replacement plan is necessary.



Table 5. Inventory of Affordable Housing

Block	Lot	# Units	Rms/Unit	Br/Unit	Gross Rent a/o 2006	Affordability Controls	Affordability Level
* 227	3	4	5	3	N/A	None	
		1	3	1	\$700	None	1 - Low Income
		3	4	2	\$411, \$500, \$750	None	4 - Low Income
227	5	6	5	3	\$447, \$609, \$790	None	5 - Low Income
					\$825, \$850, \$999	None	1 - Moderate Income
227	10	8	4	2	N/A	None	
** 227	11	8	4	2	\$561, \$772, \$790, \$801, \$806	None	8 - Low Income
		3	4	2	N/A	None	
		1	3	1	N/A	None	
		1	6	3	N/A	None	
228	11	4			N/A	None	0
228	13	2	4	2	*** \$885	None	2 - Low Income
243	11	2	4	2	N/A	None	
243	25	2	4	2	\$935, \$1,010	None	1 - Low; 1 - Moderate
		1	4	2	\$1,218	None	1 - Moderate Income
		1	6	3	\$1,241	None	1 - Moderate Income
		1	3	1	N/A	None	
		1	4	2	N/A	None	
243	45	2	4	2	\$583, \$572	None	1 - Low Income

All units have been occupied within the last 18 months.

- * Block 227 Lot 3 (79 Washington Street) includes 1 Section 8 voucher tenant.
- ** Block 227 Lot 11 (57 Washington Street) includes 1 Section 8 voucher tenant.
- *** The rent for this unit was \$695 exclusive of heat, as of 2001. We assumed a 3% annual increase and adjusted for heat expenses to bring the gross rent to 2006 levels.

Public Financing and Incentives

As part of this Redevelopment Plan, a Financing Programs resource guide has been developed to assist property owners, developers and other interested parties in financing potential projects. The resource guide provides a detailed outline of various grants, loans, and incentive programs applicable to development in the Township of Bloomfield. The summarized programs are designed to increase the overall financial feasibility of development projects from the perspective of the municipality, developers and investors. The summary of each program includes a general background, eligibility guidelines, and types and amount of funding available. These programs are offered by a variety of government agencies and non-profit foundations at local, state and federal levels. The programs are categorized by eligible uses. A brief description of each category is listed below:

Predevelopment – Studies and Planning

These programs provide funds for various types of market and feasibility studies, including green design and energy modeling, as well as preliminary planning and related soft costs.

Predevelopment – Acquisition

Programs with funds allocated for land acquisition and other pre-construction costs such as demolition or remediation.

Streetscape Improvements

The programs listed in this category finance streetscape activities that promote access to various types of transportation and promote a pedestrian-friendly environment.

Green Building

These programs provide financial incentives for green building techniques, with emphasis on energy-efficient technologies and renewable energy systems.

Affordable Housing

These incentives and loans facilitate the development of well-designed affordable housing.

Economic Development

The programs listed in this category encourage the participation of businesses in the Bloomfield Center Redevelopment by providing funds for working capital and the purchase of fixed assets.

General Financing

These programs supply funds to finance various project costs including, but not limited to, infrastructure improvements, predevelopment costs, and construction.

The Long Term Tax Exemption Law (N.J.S.A. 40A:20-1 et. seq.) authorizes municipalities to enter into agreements with redevelopers to provide certain tax exemptions for projects within that portion of the BCRD-I which have been designated as in “need of redevelopment” – as distinguished from those designated as “in need of rehabilitation”.

The Five-Year Exemption and Abatement Law (N.J.S.A. 40A:21-1 et. seq.) authorizes short term abatements/exemptions for properties which have been designated as “in need of rehabilitation” – as distinguished from those designated as “in need of redevelopment”, if the municipality has adopted an ordinance to authorize such favorable tax treatment. The Mayor and Council have authorized its consultants to explore the advisability of authorizing such a program in certain portions of the Township.

Note that the resource guide is intended to be a comprehensive analysis of available programs. However, there may be other available programs that are not mentioned in the report. In addition, the information cited in the document is subject to change due to the availability of funds and modification of the available programs.

Project Approvals Process

The provisions of this Redevelopment Plan change the process by which approvals for the development of land are typically granted. For a typical development application, a property owner submits plans and an application to the Planning Board or Zoning Board of Adjustment, and the Board schedules a hearing on the application. However, in order to comply with this Redevelopment Plan and to streamline the review process, the following procedure will be followed: Proposed redevelopment projects that include intent and specifications that would lead to USGBC LEED certification, including a preliminary LEED checklist and anticipated rating, will receive priority attention at all steps of the approval process.

Mayor and Council Review

The Mayor and Council acting as the Redevelopment Entity shall review all proposed redevelopment projects within the redevelopment area to ensure that such project(s) is consistent with the Redevelopment Plan and relevant redeveloper agreement(s). Such review shall occur prior to the submission of the redevelopment project(s) to the Planning Board. As part of its review, the Mayor and Council may require the redeveloper(s) to submit proposed site plan applications to a subcommittee of the Mayor and Council prior to the submission of such applications to the Plan-

ning Board. Such Committee may include members of the Mayor and Council and any other members and/or professionals as determined necessary and appropriate by the Mayor and Council. Such Committee shall make its recommendations to the Mayor and Council.

In undertaking its review, the Mayor and Council shall determine whether the proposal is consistent with this Redevelopment Plan and relevant redeveloper agreement(s). In addition, the review may address the site and building design, and sustainability elements of the project to ensure that the project adequately addresses the goals and objectives of the plan.

The Mayor and Council may determine to set forth an alternate procedure to streamline the process and dispose with certain requirements with respect to projects that consist solely of repair, renovation or façade improvements of existing buildings.

Planning Board Review Process

After the designated redeveloper and the Township have executed a redevelopment agreement, all development applications shall be submitted to the Township of Bloomfield Planning Board through the normal site plan and subdivision procedures as outlined in N.J.S.A. 40:55D-1 et. seq.

The Planning Board shall deem any application for redevelopment and/or rehabilitation for any property subject to this Redevelopment Plan incomplete if the applicant has not been designated by the Mayor and Council as a redeveloper(s). Additionally, the Planning Board shall deem any application for redevelopment and/or rehabilitation for any property subject to this Redevelopment Plan incomplete if the applicant has not received approval from the Mayor and Council or any committee that it may designate for such purpose, stating that the application is consistent with the Redevelopment Plan and Redevelopment Agreement.

Neither the Planning Board nor the Board of Adjustment shall grant any deviations from the terms and requirements of this Redevelopment Plan, including the granting of any ‘c’ or ‘d’ variances. Any proposed changes to the Redevelopment Plan shall be in the form of an amendment to the Redevelopment Plan adopted by the Mayor and Council in accordance with the procedures set forth in the Local Redevelopment and Housing Law, N.J.S.A. 40A:12A-1 et. seq.



Amending the Redevelopment Plan

This Redevelopment Plan may be amended from time to time in accordance with the procedures of the Redevelopment and Housing Law. To the extent that any such amendment materially affects the terms and conditions of duly executed redevelopment agreements between one or more redevelopers and the Township of Bloomfield, the provisions of the Redevelopment Plan amendment will be contingent upon the amendment of the redeveloper agreement to provide for the plan amendment.

Certificate of Completion and Compliance

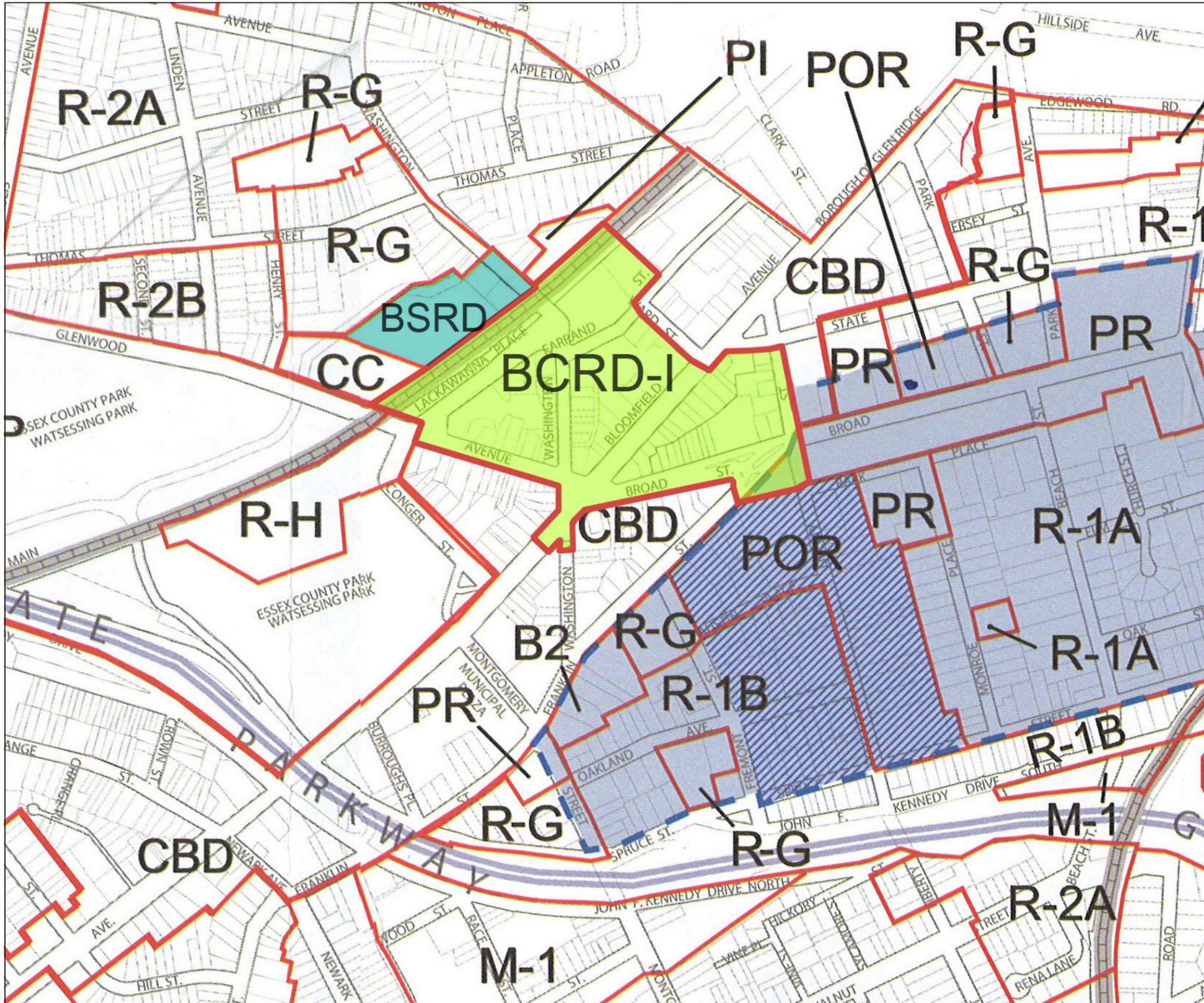
Upon the inspection and verification by the Mayor and Council that the redevelopment of a parcel subject to a redeveloper agreement has been completed, a Certificate of Completion and Compliance will be issued to the redeveloper and such parcel will be deemed no longer in need of redevelopment.

This Redevelopment Plan will remain effective until the RPA has been redeveloped and deemed no longer in need of redevelopment by the Mayor and Council.

Severability

The provisions of this Redevelopment Plan are subject to approval by Ordinance. If a Court of competent jurisdiction finds any word, phrase, clause, section, or provision of this Redevelopment Plan to be invalid, illegal, or unconstitutional, the word, phrase, clause, section, or provision shall be deemed severable, and the remainder of the Redevelopment Plan and implementing Ordinance shall remain in full force and effect.





RESIDENTIAL		
R-1A	SINGLE FAMILY	50 FOOT FRONTAGE
R-1B	SINGLE FAMILY	40 FOOT FRONTAGE
R-2A	TWO FAMILY	50 FOOT FRONTAGE
R-2B	TWO FAMILY	40 FOOT FRONTAGE
R-G	GARDEN APARTMENT	
RT	RESIDENTIAL TOWNHOUSE	
R-H	HIGH RISE APARTMENT	
POR	PROFESSIONAL OFFICE / RESIDENTIAL	
CORD	COMMUTER ORIENTED RESIDENTIAL DISTRICT	
PUBLIC		
PR	PUBLIC / RECREATIONAL	
PI	PRIVATE / INSTITUTIONAL	
BUSINESS		
B-2	NEIGHBORHOOD BUSINESS	
CBD	CENTRAL BUSINESS DISTRICT	
CC	COMMUNITY COMMERCIAL	
RO	REGIONAL OFFICE	
CD	COLLEGE DISTRICT	
INDUSTRIAL		
M-1	GENERAL INDUSTRIAL	
SPECIAL DISTRICTS		
[Blue Hatched Box]	HISTORIC DISTRICT OVERLAY	
[Teal Box]	BLOOMFIELD STATION REDEVELOPMENT DISTRICT	
[Blue Box]	COLLEGE DISTRICT OVERLAY	
[Light Green Box]	BLOOMFIELD CENTER REDEVELOPMENT DISTRICT - PHASE I (BCRD-I)	

SCALE IN FEET
0 600 1,200

Schedule A:
Zoning Map